

## COMMUNITY DEVELOPMENT DEPARTMENT

September 30<sup>th</sup>, 2019 Meeting

---

**SYNOPSIS:**

A site plan, preliminary plat, and final plat have been submitted for the Johnston Town Center development. The proposed development includes the resubdivision of approximately 16.55 acres into 9 lots, 1 outlot, and 4 right-of-way parcels. The development also includes the common shared amenities for the Johnston Town Center, a park/trail head, development of a new city hall building, and a structure to house mechanical equipment for shared common amenities. Private buildings would be detailed with future site plans.

RECOMMENDATION:

Staff recommends approval and provides the following motion for the commission's consideration:

**The Planning & Zoning Commission recommends approval of PZ Case No. 19-25; the site plan, preliminary plat, and final plat for the Johnston Town Center development subject to the following conditions:**

1. The project shall be in conformance and in accordance with the requirements, standards and regulations of the City of Johnston, and any other requirement of state or federal law or administrative rule.
2. A Stormwater Management Facilities Maintenance Agreement shall be provided and accepted by the City Council prior to approval of a site plan for any private building within the development.
3. The Johnston Town Center is to be developed with common open space areas. Thus, individual buildings shall not be required to provide open space which is in addition to the open space provided as part of the common Johnston Town Center development.
4. Provided the cumulative total parking requirement for any use within the Johnston Town Center does not exceed the specified Shared Parking Allowance established in the Johnston Town Center Shared Parking Plan, individual buildings will not be required to provide on-street or off-street parking which is in

- addition to the parking provided as part of the common parking facilities for the Johnston Town Center.
5. Additional parking may be required at the discretion of the city council if the cumulative total parking requirement for any use exceeds the specified Shared Parking Allowance established in the Johnston Town Center Shared Parking Plan.
  6. The bicycle parking facilities identified on the site plan are intended to serve the entire Johnston Town Center Development. Thus, individual buildings will not be required to provide bicycle parking which is in addition to the common bicycle parking facilities for the Johnston Town Center.
  7. Additional bicycle parking may be required at the discretion of the City Council if demand for bicycle parking exceeds the provided facilities within the development.
  8. The shrub/grass cluster streetscape requirement shall be deferred until the time of development of the adjoining building or parking areas.
  9. Maintenance bonds for public improvements shall be provided to the City prior to issuance of a building permit on any private development parcel.
  10. **The site plan and preliminary plat for the Johnston Town Center development shall be amended to address the following items:**
    - a. Address all comments from Johnston Community Development's memo dated September 26<sup>th</sup>, 2019.
    - b. Address all comments from Johnston Public Work's memo dated September 27<sup>th</sup>, 2019.
    - c. Address all comments from Foth Infrastructure and Environment's memo dated September 26<sup>th</sup>, 2019.
  11. **Prior to City Council approval of the Construction Plans for any phase of the development, the following items shall be included on the Construction Plans:**
    - a. The Construction Plans and subsequent Final Plat(s) shall include critical spot elevations for all drainage ways constructed throughout the site. As built elevations shall be provided for these areas prior to the City's acceptance of public improvements to verify construction at the appropriate elevations.
    - b. The final location of all Post Office Cluster Box Units shall be identified and the necessary concrete pads and sidewalk connections shall be installed with the public improvements.
  12. **The Final Plat for Johnston Town Center shall be amended to address the following items:**
    - a. Provide an engineer exhibit which identifies the 100 year water levels and any critical spot elevations for drainageways.

b. Address all comments from Foth Infrastructure and Environment's memo dated September 26<sup>th</sup>, 2019.

---

Motion by \_\_\_\_\_, seconded by \_\_\_\_\_ to recommend approval of PZ Case No. 19-25; the site plan, preliminary plat, and final plat for the Johnston Town Center development subject to conditions as outlined in the staff report.

Attachments:      Aerial Vicinity Map;  
                            Section 168.12, Johnston Zoning Ordinance;  
                            Merle Hay Road Town Square Overlay Zoning District Design Guidelines;  
                            Johnston Town Center Site Plan / Preliminary Plat;  
                            Johnston Town Center Final Plat;  
                            City Hall Architectural Elevations;  
                            Site Support Building Architectural Elevations;  
                            Community Development's Review Comments, dated September 26<sup>th</sup>, 2019;  
                            Public Works' Development Review Comments, dated September 27<sup>th</sup>, 2019;  
                            Foth Infrastructure & Environment's Development Review Comments, dated September 26<sup>th</sup>, 2019;  
                            Neighborhood Mailing List;

PROPERTY OWNER:	City of Johnston 6221 Merle Hay Road Johnston, IA, 50131
DEVELOPER:	Hansen JTC, LLC 5665 Greendale Road Suite A Johnston, IA, 50131
REPRESENTATIVE:	Confluence, Inc. 525 17 <sup>th</sup> Street Des Moines, IA, 50309
BACKGROUND & PRIOR APPROVALS:	<p>The first comprehensive plan for the City of Johnston was prepared in 1973. This plan included a future land use map, just as our current comprehensive plan does. The future land use designation for the town center site was identified to be general commercial and limited commercial along the western edge of the site and medium density residential along the eastern edge of the site. Also, within this plan was the desire to construct two shopping centers to service local and regional shoppers. One shopping center location was to be located at the southern end of Merle Hay Road near the interstate. The other shopping center was to be located at the northern end of Merle Hay Road near the intersection of NW Beaver Drive.</p> <p>The city updated the comprehensive plan in 1984. This plan update acknowledged the creation of one new shopping center along Merle Hay Road (village square mall / Hy-Vee). The future land uses map was updated to reflect that the town center site would become retail and service commercial on the west side and medium density residential on the east side.</p> <p>In 1992 the city updated the comprehensive plan again. This plan update indicated a future land use designation of retail &amp; service commercial on the western side of the town center site. The eastern half of the site did not have a future land use designation identified, but was shown as an existing land use of single family residential. In the 1992 plan there was a policy that commercial land uses should be developed in clusters rather than continuous strip type development. These clusters should be located at major intersections.</p> <p>The fourth plan for the city was completed in 1998. This plan had a guiding principal of “a place for people to gather”. This guiding principal was the first specific mention of creating a “town square” in Johnston. Within the 1998 plan the idea of a community node at the intersection of NW 62<sup>nd</sup> Avenue and Merle Hay Road emerged as well as a second node and NW 62<sup>nd</sup> Avenue and NW 86<sup>th</sup> Street. The plan stated, “Both of these nodes will feature a mixed use development pattern consisting of higher density housing, commercial uses, public spaces, and offices. Together they will</p>

become the central gathering places for Johnston.

Further within the 1998 plan a new future land use emerged. This was the first iteration of the Mixed-Use future land use within the city's comprehensive plan. The two previously mentioned community nodes were identified as mixed use. Within the mixed-use future land use designation there was discussion about making these areas pedestrian friendly rather than solely focusing on vehicular movement. The plan provided details on connecting buildings to the sidewalk network, making the crossing of streets more comfortable with pedestrian crossing zone improvements, buildings should have smaller footprints and have well-articulated facades, and finally land uses should be in close proximity to one another and in some instances occurring on the same block or possibly within the same building.

In response to the 1998 comprehensive plan, the city created a new zoning district known as the Mixed Use Center Zoning District in 2000. This zoning district was and still is intended to permit a mix of land uses in close proximity. The western half of the town center site was rezoned to MUC-ROC-2 with the creation of the Mixed Use Center zoning district. The eastern half of the town center site remained within the R-1(75) zoning district at that time.

In 2007 the city undertook a planning process which concluded with a plan titled "The Merle Hay Road Redevelopment Study". This planning area of the 2007 study included property from NW 60<sup>th</sup> Avenue in the south to NW 66<sup>th</sup> Avenue in the north and from NW 59<sup>th</sup> Court in the west to Lawson Elementary School in the east. The study area totaled approximately 156 acres. This plan called for a mix of land uses within the newly formed Mixed Use Center Zoning District.

Some of the early successes of this plan were the development of the Public Safety Building and Cornerstone Commons Senior Apartments. Subsequent investment continued to occur with the construction of the Cadence Apartment Complex, 62W Apartment Complex, Fareway Grocery Store, and most recently the ongoing construction of the Bricktowne Johnston Apartment Complex.

Beginning in 2013, the city began conducting community surveys to assess satisfaction with the delivery of major city services to help set community priorities so that tax dollars are spent wisely. Subsequent additional surveys were conducted in 2015 and 2018. The surveys conducted gathered responses on opinions of city growth and economic development.

Notable responses from the survey in 2013 indicated that 89% of the residents surveyed reported that they typically go outside the Johnston city limits to shop once a week or more and that 79% of the residents surveyed

felt that the city should put an emphasis on bringing retail and restaurants to the city.

Notable responses from the survey in 2015 indicated that 90% of the residents surveyed reported that they typically go outside the Johnston city limits to shop once a week or more and that 79% of the residents surveyed felt that the city should put an emphasis on bringing retail and restaurants to the city. The 2015 survey also indicated that 75% of the residents surveyed would like to see casual dining in Johnston. The other types of retail and restaurant options that residents would most like to see in Johnston are: specialty shops (58%), clothing stores (44%), and fine dining (41%).

Notable responses from the survey in 2018 indicated that 88% of the residents surveyed reported that they typically go outside the Johnston city limits to shop once a week or more and that 72% of the residents surveyed felt that the city should put an emphasis on bringing retail to the city while 83% felt that the city should put an emphasis on bringing restaurants to the city. The 2018 survey also indicated that 80% of the residents surveyed would like to see casual dining in Johnston. The other types of retail and restaurant options that residents would most like to see in Johnston are: specialty shops (56%), fine dining (42%), and clothing stores (39%).

In 2017, as a result of the successes of the implementation of the 2007 Merle Hay Road Redevelopment Plan and the responses from the 2013 and 2015 community surveys, the city undertook an update to the Merle Hay Road Redevelopment Plan to focus on the portion of the original planning area that had been called out as a potential new town center. Throughout the planning process for the Merle Hay Road Redevelopment Plan update the city completed various public outreach activities. Activities included public open houses, surveys, public meetings and informational booths held at Jamming in Johnston, Kites on the Green, Farmer's Market and other community events.

Two surveys were completed as part of the planning process. The first paper survey made available at the library, city hall and at an open house focused primarily on written feedback regarding site amenities that respondents wanted a new town center to include as well as site design. The city received 30 responses to the survey. The top five preferred amenities were; restaurants, public space, farmer's market, trail connections, and water features. There was also near unanimous support for a Town Square concept over alternative main street style or open market concepts. The second online survey was completed near the end of the planning process with 129 responses received. Results from this survey indicated that 80% of respondents supported the City Council moving forward with the Town Center concept.

The planning process concluded with an update to the Merle Hay Road

**COMPREHENSIVE  
PLAN:**

Redevelopment Study which included a conceptual town center development and a draft set of design guidelines which the city could implement with a zoning overlay district in the future.

The current Johnston 2030 Comprehensive Plan was adopted December 6, 2010 and last amended on June 3<sup>rd</sup>, 2019. The subject properties are identified on the future land use map as Community Mixed Use (CMU).

“Community mixed-use areas are intended to provide a unique mix of commercial, residential, public and related uses in a pedestrian friendly environment. It is envisioned that CMU areas will include a mix of retail and service commercial, office, institutional, higher density residential, public uses an/or park and recreation uses. CMU areas are expected to be integrated both vertically and horizontally.”

As proposed, this development is consistent with the Comprehensive Plan’s future land use map as well as the Merle Hay Road Redevelopment Plan.

**ZONING DISTRICT  
& BULK  
REGULATIONS:**

The subject property is zoned MUC-ROC-2 with Merle Hay Road Town Square Overlay Zoning District. Attached to this staff report is a copy of Section 168.12 of the Johnston Zoning Ordinance which regulates the Mixed Use Center zoning district. Also attached to the staff report is a copy of the design guidelines which have been adopted as part of the Merle Hay Road Town Square Overlay Zoning District.

**TREE REMOVAL:**

Per City Ordinance, all trees 6-inch caliper diameter are automatically designated as protected trees. Protected tree status may be revoked by the City Council if tree removal is included and noted on the site plan or preliminary plat. The project plans identify multiple trees which are 6-inch caliper or larger to be removed. There has been some preliminary tree removal on the site which had been approved by the City Council. The trees removed thus far are intended to be milled and used within the new city hall building for furniture and other uses.

11 trees (9 of which are 6-inch caliper or larger) are identified to be preserved. Credit towards landscaping requirements is being provided for preserved existing trees.

**VEHICLE ACCESS &  
CIRCULATION:**

Vehicle access to the Johnston Town Center development will occur via two access points on NW 62<sup>nd</sup> Avenue, one access point on Merle Hay Road, and two access points on NW 63<sup>rd</sup> Place. Internal to the site will be a series of drive aisles with parking. The internal drive aisles will provide connection between NW 62<sup>nd</sup> Avenue and NW 63<sup>rd</sup> Place, but these drive aisles are not intended to serve as through streets. Therefore, the drive aisles are off set on either side of the development to discourage the through movement of traffic

**VEHICLE PARKING  
& LOADING ZONES:**

Per the Design Guidelines as part of the site plan the City Council has the ability to accept an alternative parking plan which would reduce or eliminate the parking requirements for individual buildings within the development. It is intended that the parking shown on the site plan will serve the entire development as a shared parking plan and therefore individual buildings would have no parking requirement as part of their approval process.

The parking plan for the site includes 488 off-street parking spaces and 34 on-street parking spaces for a total of 522 parking spaces. The site plan proposes utilizing a shared parking factor. Shared parking acknowledges that certain uses have off-peak parking demands and therefore a parking space may be available for use by multiple buildings/uses throughout the day. For example, office uses are typically an 8 AM to 5 PM use while hotels are typically a late evening to early morning uses. Below is the proposed shared parking plan for the Johnston Town Center site:

Johnston Town Center Shared Parking Plan			
Use	Development Assumptions	Shared Parking Factor <sup>(1)</sup>	Shared Parking Allowance <sup>(2 and 3)</sup>
Restaurant, Bar, or Coffee Shop	18,500 Square Feet (does not include outdoor patio areas or common weather protected pavilions)	1.2	155
Retail and Office	53,000 Square Feet	1.2	177
Retail and Office	48,000 Square Feet	1.7	113
Boutique Hotel	80 Guest Rooms, 4,000 Square Feet Accessory Use, and 20 Employees	1.7	77
<b>Total Parking Provided:</b>			<b>522</b>
<p>1. Shared Parking Factors have been used to provide credit towards the standard parking requirements for the Johnston Town Center. The shared parking factors acknowledge that certain businesses are anticipated to be off-peak from one another, bicycle parking facilities are being provided in multiple easily accessible locations, pedestrian access has been promoted and enhanced for ease of use and safety, and access to public transportation is available.</p>			
<p>2. Provided the cumulative total parking requirement for any use within the Johnston Town Center does not exceed the specified Shared Parking Allowance established in the Johnston Town Center Shared Parking Plan, individual buildings will not be required to provide on-street or off-street parking which is in addition to the parking provided as part of the common parking facilities for the Johnston Town Center.</p>			



3. Additional parking may be required at the discretion of the city council if the cumulative total parking requirement for any use exceeds the specified Shared Parking Allowance established in the Johnston Town Center Shared Parking Plan.

**PEDESTRIANS,  
BICYCLISTS, AND  
PUBLIC TRANSIT:**

Per the Design Guidelines, a pedestrian circulation plan is required with site plans. The use of special pavement treatments and markings to delineate and announce pedestrian crossings is required. As part of the development there will be a network of sidewalks and wider paths internal of the development. Along Merle Hay Road there will be a 10 foot wide trail installed with the roadway improvements and along NW 62<sup>nd</sup> Avenue there will be an 8 foot wide trail installed with the roadway improvements.

Per the Design Guidelines, every building that contains residential and retail uses including restaurants, bars, and coffee shops will be required to have access to a shared or common bike parking facility. As part of this site plan six bike parking locations have been identified throughout the development. City hall will include 5 bike racks with capacity for 10 bicycles. The parkland area to the east of city hall will include 14 bike racks with capacity for 28 bicycles. The parkland area will also include 1 bike fix it station. The promenade area adjoining the main vehicle entrance on Merle Hay Road will include 10 bike racks with capacity for 20 bicycles. This promenade area also identifies the possibility for 26 future bike racks for an added bicycle capacity of 52 bicycles. Finally, the northwest pedestrian entrance to the site will include 7 bike racks with capacity for 14 bicycles. The northwest site entrance will also include a bike fix it station.

All combined, the site will have the initial bicycle parking capacity at bike racks for 72 bicycles. With future bike parking there would be capacity at bike racks for 124 bicycles. This bike parking is intended to serve the bike parking needs for the entire site thus eliminating the requirement for individual sites to install bike parking. With the future build-out of the site and any future changes of uses the city will need to monitor the demand for bike parking and the proximity of uses to the location of the bike parking facilities. If future demand shows a need for additional bike parking then individual buildings may be required to install additional bike parking facilities at the discretion of the City Council.

While not specifically addressed within the zoning code or the Design Guidelines, public transportation is available to the development via DART Bus Route #5. Staff has been working with DART to identify the new location for the bus stop as a result of improvements to Merle Hay Road adjoining the site. The plan is to locate a bus stop at the central entrance to the development on Merle Hay Road and to locate a bus stop with a bus shelter at the northwest corner of the intersection of Merle Hay Road and NW 62<sup>nd</sup> Avenue for southbound riders.

UTILITIES:

**Sanitary Sewer:** The development will be serviced by sanitary sewer. As a separate project, the City of Johnston will be completing improvements to Merle Hay Road and NW 62<sup>nd</sup> Avenue. This site will utilize new sanitary sewer to be constructed as part of that separate project.

**Water:** The development will be serviced by water mains. As a separate project, the City of Johnston will be completing improvements to Merle Hay Road and NW 62<sup>nd</sup> Avenue. This site will utilize new water mains to be constructed as part of that separate project.

**Storm Sewer:** The development will be serviced by storm. As a separate project, the City of Johnston will be completing improvements to Merle Hay Road and NW 62<sup>nd</sup> Avenue. This site will utilize new storm sewer to be constructed as part of that separate project.

FIRE PROTECTION:

A series of fire hydrants will be installed within the site to provide fire coverage to all buildings. City hall will include a sprinkler system. Also, city hall and site support building will include a knox box for emergency personal access.

DRAINAGE:

The site is generally draining from east to west towards the 72-inch storm sewer along NW 59<sup>th</sup> Court. The stormwater practices called for as part of this development account for all of the stormwater quality and quantity needs for the entire area within the Johnston Town Center development. Any area outside of the boundaries of the Johnston Town Center development have not been accounted for as part of this development. Therefore, should any areas outside of the boundaries of the Johnston Town Center choose to develop then the stormwater management needs for those sites shall be reviewed and additional facilities installed as needed.

The vast majority of the stormwater management on the site is being managed in underground chambers. This practice allows for the maximum utilization of the site for development purposes. This practice will provide for both stormwater quality needs and stormwater quantity needs.

To further assist with addressing stormwater quality volumes, this development will utilize permeable pavers, uncompacted tree pits, and bio-retention cells. These practices will have the added benefit of being demonstration sites to future developers in the city and the region as to how stormwater may be managed on a site in various manners. With the utilization of these practices this development not only meets the requirements for water quality volume, but actually exceeds these requirements.

Finally, there will be above ground detention basins along the eastern edge of the site within the parkland area. These areas will be landscaped in a manner to soften the appearance of the basins. There has been a strong

desire to make sure that these basins are able to still be used as parkland in a manner other than just stormwater detention. Currently being explored is the possibility of utilizing the basins for a bicycle pump track as the slopes of the basin could compliment that activity.

#### OPEN SPACE:

Per the Merle Hay Road Town Square Design Guidelines 30% of the site must be maintained as open space. It is intended that the provision allowing for the creation of common open space will be utilized which in turn eliminates the need for individual buildings to provide open space. For this site the open space requirement is calculated as follows:

615,218 SF X 30% Open Space Requirement = 184,566 SF Open Space

There are two main areas of open space provided on the site. The first area is the area just west of City Hall, which is to be called “The Yard”, the second area is the eastern park to the east of City Hall. These areas are designed to be gathering places for the community and visitors of the development. Each will be activated in their own ways to promote social interaction.

The area west of City Hall will contain a plaza which in the warmer months will be used for outdoor seating and contain a splash pad in the center. “The Yard” will also include a large grassy area which can be used for a variety of events. It is expected that the farmer’s market will setup either in or adjoining this area. In warmer months this area could also be used for activities such as movie in the park or other entertainment type events. In the cooler months “The Yard” will be transformed into an ice rink for recreational skating, broomball, and will include two curling lanes.

The eastern park is intended to function as a bike trailhead. This area will be heavily landscaped in accordance with the buffer requirements of the town center and contain stormwater basins. The basins depth and configuration are designed to provide a natural feel to the extent that will still allow for substantial storage capacity. Native plantings are to be planted in portions of the basins particularly areas with steep slopes and or are most likely to occasionally experience standing water. The basin design also considers the possibility of a bicycle pump track that is currently being explored by Confluence and the Parks Department. The city may add additional park features to this area in the future, but demand for facilities and activities will dictate how this area is developed in the future.

#### BUFFERING:

Per the Design Guidelines, the following buffers are required on the site:

- A permanent 50 foot buffer along the east boundary of the development site.
- A permanent 30 foot buffer along the northern development boundary when adjoining single family residential.

- Temporary buffers between the development and any single family residential located within the Merle Hay Road Town Square Overlay Zoning District.

The following are the buffer materials required and those provided:

<b>Required 30' Buffer</b>		
<i>Buffer Material</i>	<i>Required</i>	<i>Provided</i>
Overstory Trees:	3	3 (1 existing)
Understory Trees:	12	9
Evergreen Trees:	9	7
Shrubs:	24	18

<b>Required 50' Buffer</b>		
<i>Buffer Material</i>	<i>Required</i>	<i>Provided</i>
Berm:	Yes	Yes
Overstory Trees:	17	17 (2 existing)
Understory Trees:	33	33
Evergreen Trees:	33	33
Shrubs:	131	131

Staff recommends the site plan be amended to identify the correct buffer materials for the required 30 foot wide buffer in the northeast corner of the development.

The following temporary buffers are proposed between the development and the single family residential which will remain within the Merle Hay Road Town Square Overlay Zoning District:

- Fencing
- Concentration of open space landscaping along the shared property boundaries.

#### LANDSCAPING:

Per the Design Guidelines there are multiple landscaping requirements for this site. These requirements include streetscape landscaping, open space landscaping, foundation landscaping, and buffer landscaping. This section will address streetscape landscaping, open space landscaping, and foundation landscaping. For a discussion regarding buffer landscaping please reference the buffer section of this staff report.

#### **Streetscape Landscaping**

The design guidelines require that 1 overstory tree or 2 understory trees be

provided for every 50 feet of street frontage or any combination thereof. Additionally, 1 cluster of shrubs and/or ornamental grasses must be provided for every 20 feet of street frontage. The streetscape landscaping is required and provided:

<b>Merle Hay Road Streetscape Landscaping</b>		
<i>Material</i>	<i>Credits Required</i>	<i>Credits Provided</i>
Overstory Trees (1 credit per 1 tree)	NA	14
Understory Trees (1 credit per 2 trees)	NA	4
Total Tree Credits	17	18
Shrub/Grass Cluster Credits (1 credit per cluster)	41	0

<b>NW 62<sup>nd</sup> Avenue Streetscape Landscaping</b>		
<i>Material</i>	<i>Credits Required</i>	<i>Credits Provided</i>
Overstory Trees (1 credit per 1 tree)	NA	27
Understory Trees (1 credit per 2 trees)	NA	18
Total Tree Credits	17	36
Shrub/Grass Cluster Credits (1 credit per cluster)	43	0

Staff does recommend that the grass/shrub clusters be deferred for identification and installation to such a time as the adjoining structures or parking areas are constructed.

### **Foundation Landscaping**

The design guidelines require that foundation landscaping be provided adjoining any building to soften the buildings transition to the site. Foundation landscaping has been identified for city hall and the site support building which is in compliance with these requirements. Additional foundation landscaping will be installed with individual private buildings. Those additional foundation plantings would be determined with future site plans.

### **Open Space Landscaping**

The design guidelines require one tree and one shrub per 1,000 square feet of required open space. In addition to the total number of trees required, the design guidelines require that 50% of the trees be overstory trees and 15%

of the trees be evergreen trees. The remaining trees can be of any variety. For this development the following open space landscaping is required and provided:

<b>Open Space Landscaping</b>		
<i>Material</i>	<i>Required</i>	<i>Provided</i>
Overstory trees	93	93
Evergreen Trees	28	28
Misc. Trees	64	61 + 92 existing*
Total Tress	185	261
Shrubs	185	570
*Credit for existing trees utilized per the Design Guidelines		

**IMPROVEMENTS TO  
ADJACENT  
ROADWAYS:**

The city is in the process of finalizing construction plans to reconstruct the intersection of Merle Hay Road and NW 62<sup>nd</sup> Avenue. These improvements will extend east -west from approximately 200 feet east of NW 54<sup>th</sup> Court to NW 59<sup>th</sup> Court. North-south improvements will extend from approximately the southern property line of the Johnston Bait and Tackle to approximately 300 feet north of NW 64<sup>th</sup> Place.

Roadway improvements will include a complete reconstruction of the intersection and the sections of NW 62<sup>nd</sup> Avenue called out. Merle Hay Road improvements will include construction of on-street parking on the east side adjoining the Town Center development and the creation of a continuous center turn lane through to NW 64<sup>th</sup> Place. Additional improvements will include reconstruction of sanitary sewer, water main, and storm sewer utilities.

The scheduled roadway project is anticipated to occur simultaneous to the initial phases of the Town Center development, but it is important to note that these two projects are separate.

**FLOOD PLAIN:**

The property is not located within a FEMA designated floodplain.

**CITY HALL  
ARCHITECTURE:**

With this site plan architectural elevations have been submitted for the new city hall facility. Per the design guidelines all buildings must be constructed with a minimum 80% of each façade being a class 1 or class 2 material. Material classes are identified in the design guidelines. Up to 20% of any façade may be a class 3 material and up to 5% of any façade may be a class 4 material.

City hall is proposed to be constructed using glass, limestone panels, and glassfibre reinforced concrete panels. As such each elevation is to be

**SITE SUPPORT  
BUILDING  
ARCHITECTURE:**

entirely class 1 and class 2 materials.

City hall is in compliance with all architectural requirements of the design guidelines.

With this site plan architectural elevations have been submitted for a site support building which would house the ice rink mechanical equipment and other maintenance equipment for the town center. Per the design guidelines all buildings must be constructed with a minimum 80% of each façade being a class 1 or class 2 material. Material classes are identified in the design guidelines. Up to 20% of any façade may be a class 3 material and up to 5% of any façade may be a class 4 material.

The site support building is proposed to be constructed using brick, glassfibre reinforced concrete panels, and wood soffit. As such each elevation is to be entirely class 1 and class 2 materials. The northern façade of the building is intended to include a mural with the actual mural art to be determined at a later date.

The site support building is in compliance with all architectural requirements of the design guidelines.

**FINAL PLAT  
SUMMARY:**

Lots:	9 Lots
Area:	16.55 Acres
Area of Streets:	0.46 Acres
Other Area Dedicated:	2.33 Acres Parkland
Outlot Area:	8.87 Acres
Net Area in Lots (excludes lots 3 & 4):	3.55 Acres
Net Average Lot Area (excludes lots 3 & 4):	24,954 SF
Length of Water Main:	3,021 LF
Length of Sanitary Sewer:	1,299 LF
Length of Storm Sewer:	6,785 LF

**FINAL PLAT LEGAL  
DOCUMENTS:**

The following documentation is required to be submitted for review by City Staff at minimum six (6) business days prior to consideration by the City Council and will be recorded with the Final Plat.

- a. Attorney's Title Opinion
- b. Polk County Treasurer's Tax Certificate
- c. Polk County Auditors Approval of Subdivision Plat Name
- d. Owner's Consent to Plat
- e. Water Main Easement
- f. Sanitary Sewer & Water Main Easement
- g. Public Utility Easement
- h. Maintenance bond(s)

PUBLIC NOTICE  
AND ADJACENT  
NEIGHBOR  
COMMENTS:

Notice has been sent to properties within 320 feet of the subject property and staff has posted a proposed development notice to the city's website. A copy of the mailing list is attached. As of publication of this report no public comments have been received.

CONSULTANT  
COMMENTS:

The project has been reviewed by our consulting engineer, Foth Infrastructure and Environment, LLC. Foth has issued review comments in a letter dated September 26<sup>th</sup>, 2019. Staff recommends a condition for approval to revise the site plan / preliminary plat and final plat to address all comments from Foth's September 26<sup>th</sup>, 2019 review letter.

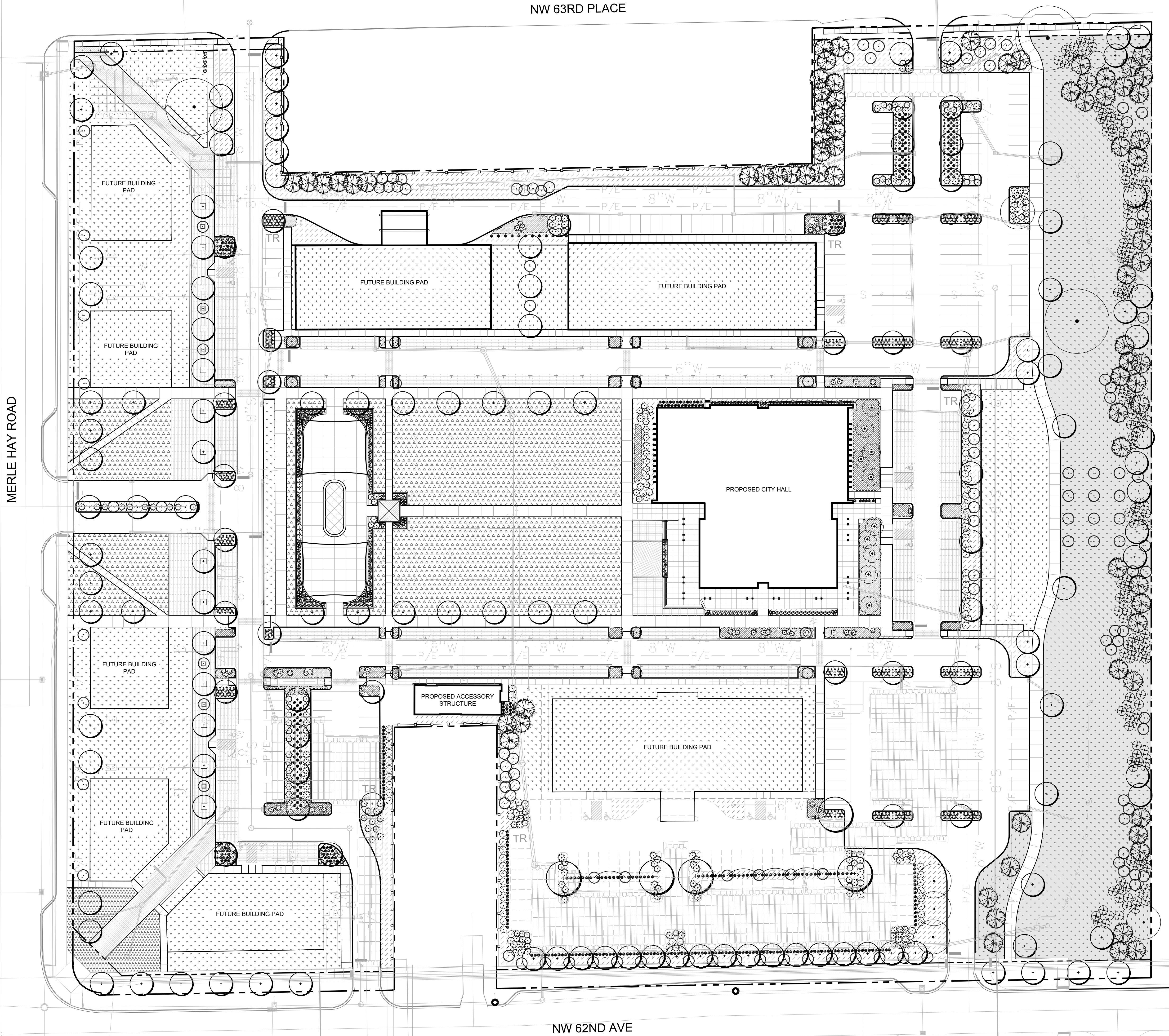






U  
—  
T  
—  
S  
—  
R  
—  
Q  
—  
P  
—  
N  
—  
M  
—  
L  
—  
K  
—  
J  
—  
H  
—  
G  
—  
F  
—  
E  
—  
D  
—  
C  
—  
B  
—  
A

1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | 15 | 16 | 17 | 18 | 19 | 20 | 21 | 22 | 23 | 24 | 25



LAYOUT NOTES:

1. ALL CURBS RAMP TO BE BUILT AS PER FEDERAL ACCESSIBILITY STANDARDS.
2. PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
3. BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM SURVEY ARE PREPARED BY CIVIL DESIGN ADVANTAGE, 3405 SE CROSSROADS DR. #G, GRIMES, IA 50111.
4. ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
5. VERIFY COORDINATES PRIOR TO CONSTRUCTION.
6. CONTRACTOR TO SUPPLY AND INSTALL ALL NECESSARY SLEEVES UNDER PAVING AND WALKS.
7. PROPOSED SIGNAGE TO MEET CITY CODES FOR GROUND SIGNS.
8. CONSTRUCT SITE IMPROVEMENTS IN ACCORDANCE TO THE IOWA SUDAS, CURRENT EDITION, AND SPECIFICATIONS.

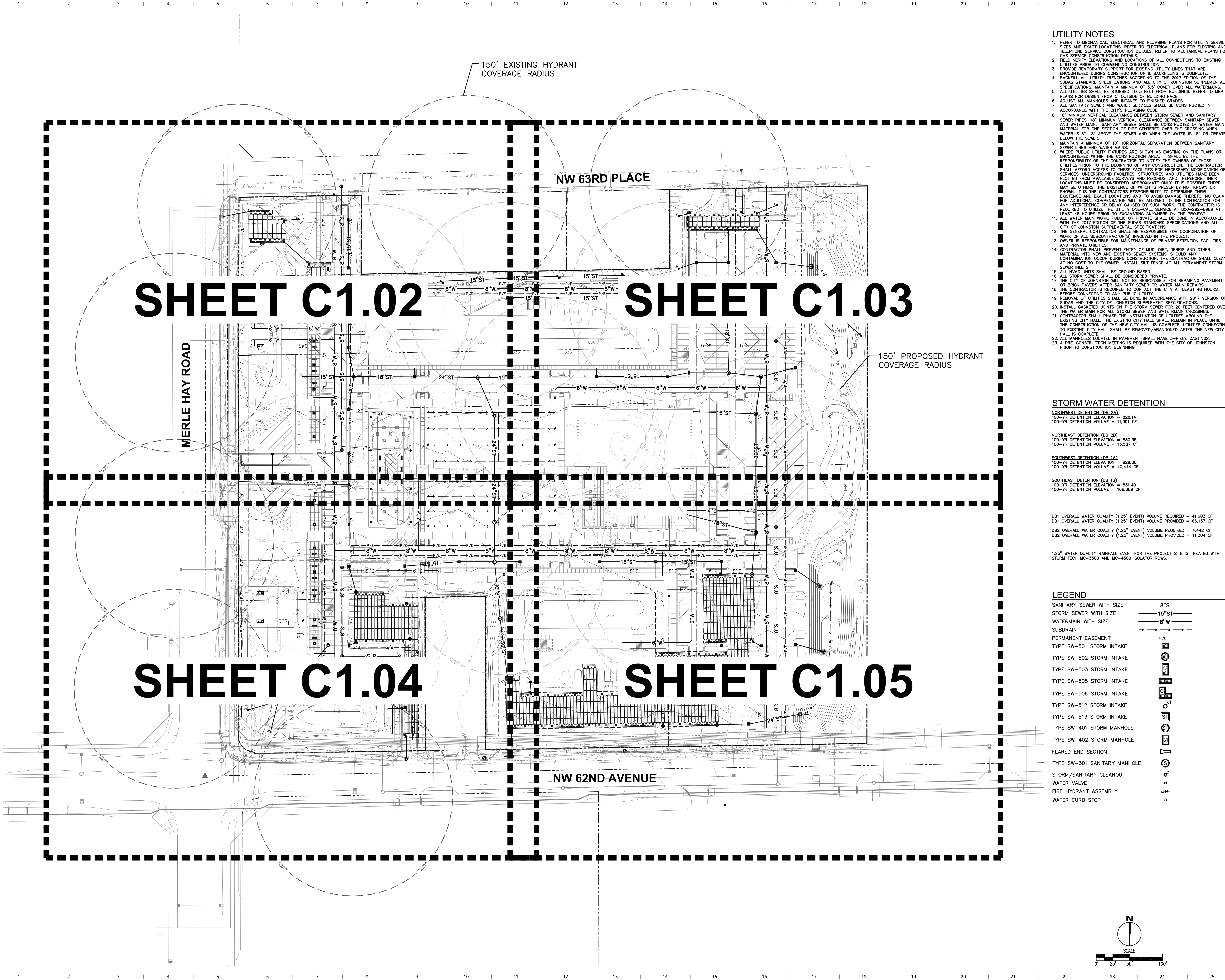
LEGEND

	TYPE 3 PCC BANDS
	TYPE 4 UNIT PAVER
	TYPE 5 UNIT PAVER
	TURF SEED
	SOD
	NATIVE SEED
	PLANTING BED



PRINTED:

U  
—  
T  
—  
S  
—  
R  
—  
Q  
—  
P  
—  
N  
—  
M  
—  
L  
—  
K  
—  
J  
—  
H  
—  
G  
—  
F  
—  
E  
—  
D  
—  
C  
—  
B  
—  
A



UTILITY NOTES

1. REFER TO MECHANICAL, ELECTRICAL AND PLUMBING PLANS FOR UTILITY SERVICE SIZES AND EXACT LOCATIONS. REFER TO ELECTRICAL PLANS FOR ELECTRIC AND TELEPHONE SERVICE. CONSTRUCTION DETAILS. REFER TO MECHANICAL PLANS FOR GAS SERVICE CONSTRUCTION DETAILS.
2. FIELD VERIFY ELEVATIONS AND LOCATIONS OF ALL CONNECTIONS TO EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION.
3. PROVIDE TEMPORARY SUPPORT FOR EXISTING UTILITY LINES THAT ARE ENCOUNTERED DURING CONSTRUCTION UNTIL BACKFILLING IS COMPLETE.
4. BACKFILL ALL UTILITY TRENCHES ACCORDING TO THE 2017 EDITION OF THE SUDAS STANDARD SPECIFICATIONS AND ALL CITY OF JOHNSTON SUPPLEMENTAL SPECIFICATIONS. MAINTAIN A MINIMUM OF 5'-5" COVER OVER ALL WATERMANS.
5. ALL UTILITIES SHALL BE STURBED TO 5 FEET FROM BUILDINGS. REFER TO MEP PLANS FOR DESIGN FROM 5' OUTSIDE OF BUILDING FACE.
6. ADJUST ALL MANHOLES AND INTAKES TO FINISHED GRADES.
7. ALL SANITARY SEWER AND WATER SERVICES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY'S PLUMBING CODE.
8. 18" MINIMUM VERTICAL CLEARANCE BETWEEN STORM SEWER AND SANITARY SEWER PIPES. 18" MINIMUM VERTICAL CLEARANCE BETWEEN SANITARY SEWER AND WATER MAIN. SANITARY SEWER SHALL BE CONSTRUCTED OF WATER MAIN MATERIAL FOR ONE SECTION OF PIPE CENTERED OVER THE CROSSING WHEN WATER IS 6"-18" ABOVE THE SEWER AND WHEN THE WATER IS 18" OR GREATER BELOW THE SEWER.
9. MAINTAIN A MINIMUM OF 10' HORIZONTAL SEPARATION BETWEEN SANITARY SEWER LINES AND WATER MAINS.
10. WHERE PUBLIC UTILITY FIXTURES ARE SHOWN AS EXISTING ON THE PLANS OR ENCOUNTERED WITHIN THE CONSTRUCTION AREA, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE OWNERS OF THOSE UTILITIES PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. THE CONTRACTOR SHALL AFFORD ACCESS TO THESE FACILITIES FOR NECESSARY MODIFICATION OF SERVICES. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE, THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THERE MAY BE OTHERS. THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTORS RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND TO AVOID DAMAGE THERETO. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK. THE CONTRACTOR IS REQUIRED TO UTILIZE THE UTILITY ONE-CALL SERVICE AT 800-292-5869 AT LEAST 48 HOURS PRIOR TO EXCAVATING ANYWHERE ON THE PROJECT.
11. ALL WATER MAIN WORK, PUBLIC OR PRIVATE SHALL BE DONE IN ACCORDANCE WITH THE 2017 EDITION OF THE SUDAS STANDARD SPECIFICATIONS AND ALL CITY OF JOHNSTON SUPPLEMENTAL SPECIFICATIONS.
12. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK OF ALL SUBCONTRACTOR(S) INVOLVED IN THE PROJECT.
13. OWNER IS RESPONSIBLE FOR MAINTENANCE OF PRIVATE RETENTION FACILITIES AND PRIVATE UTILITIES.
14. CONTRACTOR SHALL PREVENT ENTRY OF MUD, DIRT, DEBRIS AND OTHER MATERIAL INTO NEW AND EXISTING SEWER SYSTEMS. SHOULD ANY CONTAMINATION OCCUR DURING CONSTRUCTION, THE CONTRACTOR SHALL CLEAN AT NO COST TO THE OWNER. INSTALL SILT FENCE AT ALL PERMANENT STORM SEWER INLETS.
15. ALL HVAC UNITS SHALL BE GROUND BASED.
16. ALL STORM SEWER SHALL BE CONSIDERED PRIVATE.
17. THE CITY OF JOHNSTON WILL NOT BE RESPONSIBLE FOR REPAIRING PAVEMENT OR BRICK PAVEDS AFTER SANITARY SEWER OR WATER MAIN REPAIRS.
18. THE CONTRACTOR IS REQUIRED TO CONTACT THE CITY AT LEAST 48 HOURS BEFORE CONNECTING TO ANY PUBLIC UTILITY.
19. REMOVAL OF UTILITIES SHALL BE DONE IN ACCORDANCE WITH 2017 VERSION OF SUDAS AND THE CITY OF JOHNSTON SUPPLEMENTAL SPECIFICATIONS.
20. INSTALL GASKETED JOINTS ON THE STORM SEWER FOR 20 FEET CENTERED OVER THE WATER MAIN FOR ALL STORM SEWER AND WATER MAIN CROSSINGS.
21. CONTRACTOR SHALL PHASE THE INSTALLATION OF UTILITIES AROUND THE EXISTING CITY HALL. THE EXISTING CITY HALL SHALL REMAIN IN PLACE UNTIL THE CONSTRUCTION OF THE NEW CITY HALL IS COMPLETE. UTILITIES CONNECTING TO EXISTING CITY HALL SHALL BE REMOVED/ABANDONED AFTER THE NEW CITY HALL IS COMPLETE.
22. ALL MANHOLES LOCATED IN PAVEMENT SHALL HAVE 3-PIECE CASTINGS.
23. A PRE-CONSTRUCTION MEETING IS REQUIRED WITH THE CITY OF JOHNSTON PRIOR TO CONSTRUCTION BEGINNING.

STORM WATER DETENTION

NORTHWEST DETENTION (DB 2A)	
100-YR DETENTION ELEVATION =	828.14
100-YR DETENTION VOLUME =	11,391 CF
NORTHEAST DETENTION (DB 2B)	
100-YR DETENTION ELEVATION =	830.35
100-YR DETENTION VOLUME =	15,587 CF
SOUTHWEST DETENTION (DB 1A)	
100-YR DETENTION ELEVATION =	829.00
100-YR DETENTION VOLUME =	40,444 CF
SOUTHEAST DETENTION (DB 1B)	
100-YR DETENTION ELEVATION =	831.49
100-YR DETENTION VOLUME =	168,889 CF

DB1 OVERALL WATER QUALITY (1.25" EVENT) VOLUME REQUIRED = 41,603 CF  
DB1 OVERALL WATER QUALITY (1.25" EVENT) VOLUME PROVIDED = 66,137 CF  
DB2 OVERALL WATER QUALITY (1.25" EVENT) VOLUME REQUIRED = 4,442 CF  
DB2 OVERALL WATER QUALITY (1.25" EVENT) VOLUME PROVIDED = 11,304 CF

1.25" WATER QUALITY RAINFALL EVENT FOR THE PROJECT SITE IS TREATED WITH STORM TECH MC-3500 AND MC-4500 ISOLATOR ROWS.

LEGEND

SANITARY SEWER WITH SIZE	8"S
STORM SEWER WITH SIZE	15"ST
WATERMAIN WITH SIZE	8"W
SUBDRAIN	→ → →
PERMANENT EASEMENT	— P/E —
TYPE SW-501 STORM INTAKE	
TYPE SW-502 STORM INTAKE	
TYPE SW-503 STORM INTAKE	
TYPE SW-505 STORM INTAKE	
TYPE SW-506 STORM INTAKE	
TYPE SW-512 STORM INTAKE	
TYPE SW-513 STORM INTAKE	
TYPE SW-401 STORM MANHOLE	
TYPE SW-402 STORM MANHOLE	
FLARED END SECTION	
TYPE SW-301 SANITARY MANHOLE	
STORM/SANITARY CLEANOUT	
WATER VALVE	
FIRE HYDRANT ASSEMBLY	
WATER CURB STOP	



100 Court Ave., Suite 100  
Des Moines, IA 50309  
P: 515-309-0722  
F: 515-309-0725  
www.opnarchitects.com

All reports, plans, specifications, computer files, field data, notes and other documents and instruments prepared by OPN Architects, Inc. are instruments of service that remain the property of OPN Architects, Inc. OPN Architects, Inc. shall retain all common law, statutory and other reserved rights, including the copyright therein.

© 2019 OPN Architects, Inc.

Owner

Project

General Contractor

Contractor Name  
XXX 1ST STREET  
DES MOINES, IA 50309  
P. 515-309-XXXX  
F. 515-309-XXXX

Landscape Architect

CONFLUENCE  
525 17TH STREET  
DES MOINES, IA 50309  
P. 515-288-4875  
F. 515-288-8359

Civil Engineer

CIVIL DESIGN ADVANTAGE  
3405 SE CROSSROADS DR. #G  
Grimes, IA 50111  
P. 515-369-4400

Structural Engineer

RAKER RHODES ENGINEERING  
4717 GRAND AVE  
DES MOINES, IA 50312  
P. 515-277-0275

Mechanical Engineer

IMEG CORP.  
2882 106TH STREET  
DES MOINES, IA 50322  
P. 515-334-9906  
F. 515-334-9908

Electrical Engineer

IMEG CORP.  
2882 106TH STREET  
DES MOINES, IA 50322  
P. 515-334-9906  
F. 515-334-9908

Key Plan

Revision Description Date

PRELIMINARY.  
NOT FOR  
CONSTRUCTION.

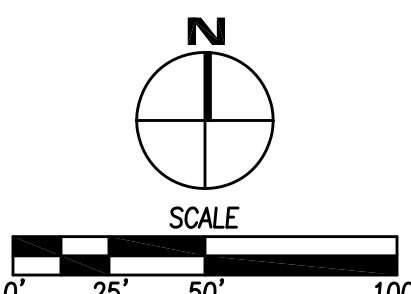
OPN Project No.  
Project Number

Sheet Issue Date  
100% DD 06/18/2019

Sheet Name

UTILITY PLAN REFERENCE

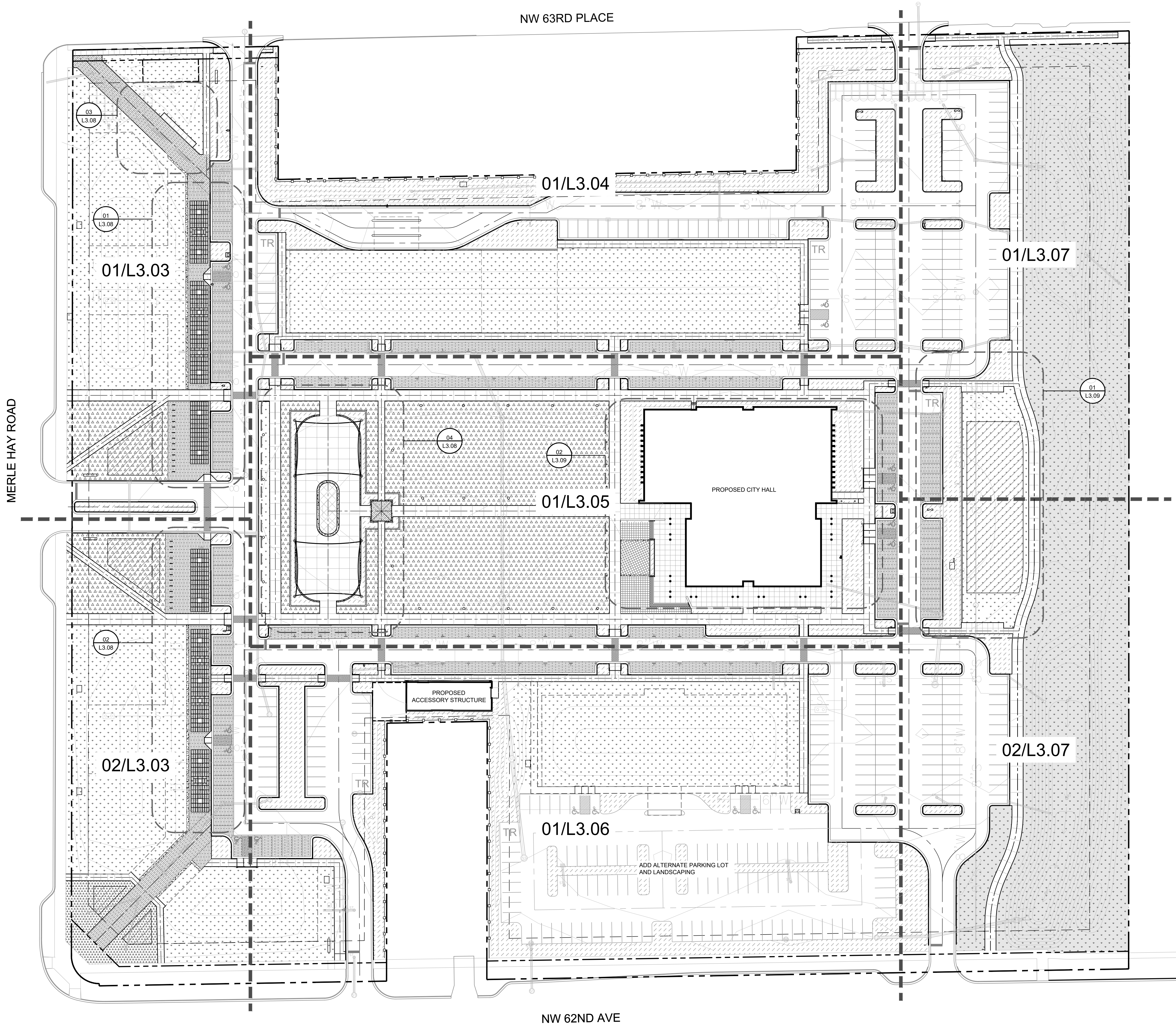
Sheet Number



C1.01



U  
—  
T  
—  
S  
—  
R  
—  
Q  
—  
P  
—  
N  
—  
M  
—  
L  
—  
K  
—  
J  
—  
H  
—  
G  
—  
F  
—  
E  
—  
D  
—  
C  
—  
B  
—  
A

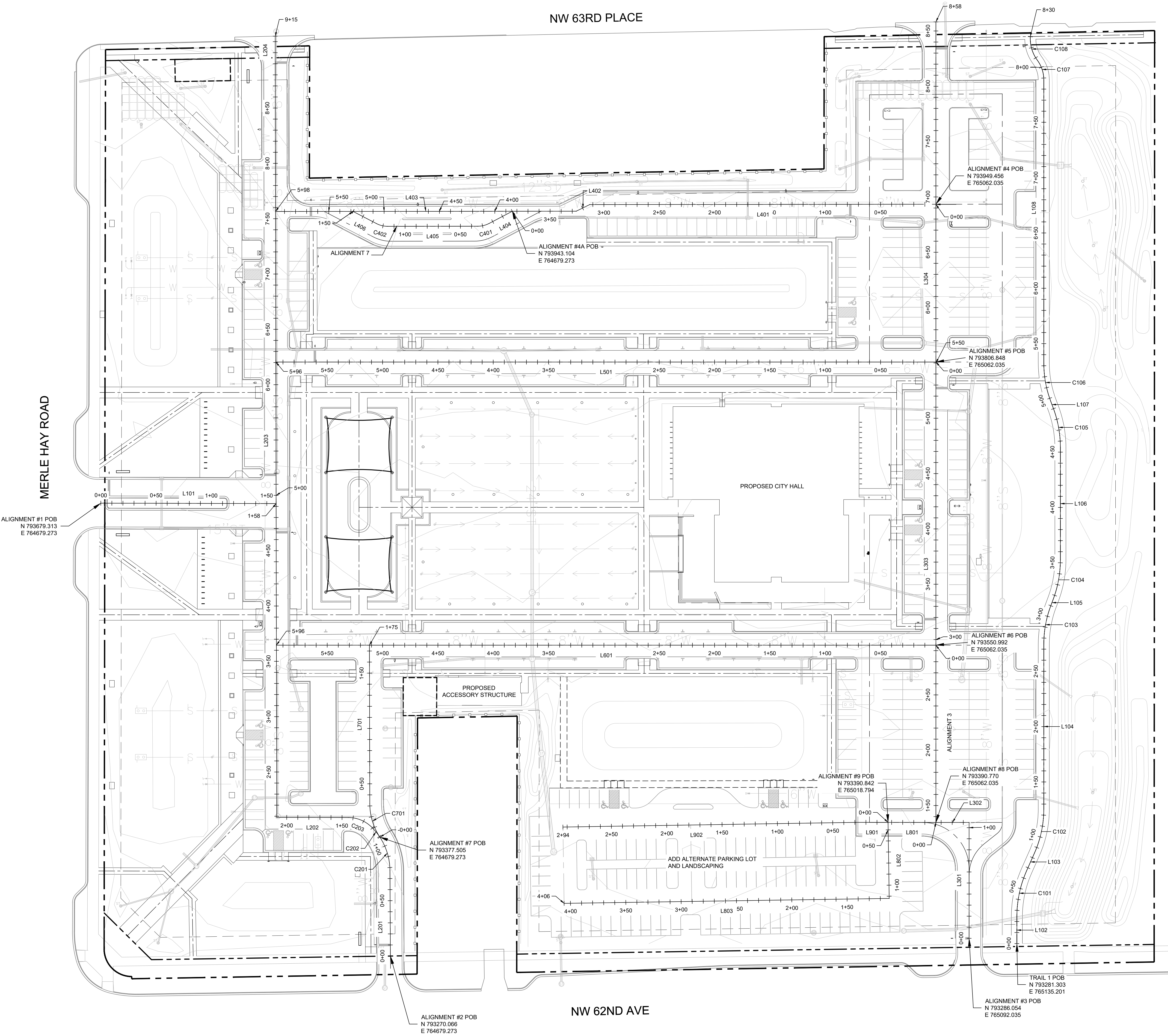


LAYOUT NOTES:

- ALL CURBS RAMP TO BE BUILT AS PER FEDERAL ACCESSIBILITY STANDARDS.
- PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
- BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM SURVEY ARE PREPARED BY CIVIL DESIGN ADVANTAGE, 3405 SE CROSSROADS DR. #G, GRIMES, IA 50111.
- ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
- VERIFY COORDINATES PRIOR TO CONSTRUCTION.
- CONTRACTOR TO SUPPLY AND INSTALL ALL NECESSARY SLEEVES UNDER PAVING AND WALKS.
- PROPOSED SIGNAGE TO MEET CITY CODES FOR GROUND SIGNS.
- CONSTRUCT SITE IMPROVEMENTS IN ACCORDANCE TO THE IOWA SUDAS, CURRENT EDITION, AND SPECIFICATIONS.



U  
  
T  
  
S  
  
R  
  
Q  
  
P  
  
N  
  
M  
  
K  
  
J  
  
H  
  
G  
  
F  
  
E  
  
D  
  
C  
  
B  
  
A



LAYOUT NOTES:

- ALL CURBS RAMPS TO BE BUILT AS PER FEDERAL ACCESSIBILITY STANDARDS.
- PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
- BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM SURVEY ARE PREPARED BY CIVIL DESIGN ADVANTAGE, 3405 SE CROSSROADS DR. #G, GRIMES, IA 50111.
- ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
- VERIFY COORDINATES PRIOR TO CONSTRUCTION.
- CONTRACTOR TO SUPPLY AND INSTALL ALL NECESSARY SLEEVES UNDER PAVING AND WALKS.
- PROPOSED SIGNAGE TO MEET CITY CODES FOR GROUND SIGNS.
- CONSTRUCT SITE IMPROVEMENTS IN ACCORDANCE TO THE IOWA SUDAS, CURRENT EDITION, AND SPECIFICATIONS.

DRIVE AISLE LINE & CURVE TABLES

Alignment 1 - West Entry					
Number	Length	Radius	Line/Chord Direction	Start Sta.	End Sta.
L101	156.43		S89° 52' 08.81"E	0+00.00	1+56.43

Alignment 2 - West Drive Aisle					
Number	Length	Radius	Line/Chord Direction	Start Sta.	End Sta.
L201	77.28		N00° 19' 01.87"W	0+00.00	0+77.28
C201	31.94	55.466	N16° 48' 57.04"W	0+77.28	1+09.22
C202	11.86	55.448	N39° 26' 39.31"W	1+09.22	1+21.09
C203	23.19	30.002	N67° 43' 14.88"W	1+21.09	1+44.28
L202	64.70		N89° 52' 08.63"W	1+44.28	2+08.98
L203	681.06		N00° 07' 06.33"W	2+08.98	8+90.03
L204	24.92		N00° 00' 00.42"E	8+90.03	9+14.95

Alignment 3 - East Drive Aisle					
Number	Length	Radius	Line/Chord Direction	Start Sta.	End Sta.
L301	104.72		N00° 00' 00.00"E	0+00.00	1+04.72
L302	30.00		N90° 00' 00.00"W	1+04.72	1+34.72
L303	416.08		N00° 00' 00.00"E	1+34.72	5+50.79
L304	307.38		N00° 00' 00.00"E	5+50.79	8+58.17

Alignment 4 - North Alley Drive					
Number	Length	Radius	Line/Chord Direction	Start Sta.	End Sta.
L401	312.77		S89° 56' 50.36"W	0+00.00	3+12.77
L402	13.67		S63° 55' 01.09"W	3+12.77	3+26.45
L403	271.67		S89° 56' 50.36"W	3+26.45	5+98.11

Alignment 4a - Drive Through					
Number	Length	Radius	Line/Chord Direction	Start Sta.	End Sta.
L404	23.72		S62° 20' 17.88"W	0+00.00	0+23.72
C401	10.60	22.000	S76° 08' 34.12"W	0+23.72	0+34.33
L405	81.38		S89° 56' 50.36"W	0+34.33	1+15.70
C402	10.55	22.000	N76° 18' 31.31"W	1+15.70	1+26.26
L406	23.87		N62° 33' 52.88"W	1+26.26	1+50.13

TRAIL LINE & CURVE TABLES

Trail Alignment 1					
Number	Length	Radius	Line/Chord Direction	Start Sta.	End Sta.
L102	24.37		N00° 00' 00.00"E	0+00.00	0+24.37
C101	42.61	102.000	N11° 57' 59.37"E	0+24.37	0+66.98
L103	17.60		N23° 55' 58.73"E	0+66.98	0+84.57
C102	40.94	98.000	N11° 57' 59.37"E	0+84.57	1+25.51
L104	149.11		N00° 00' 00.00"E	1+25.51	2+74.61
C103	35.16	98.000	N10° 16' 40.88"E	2+74.61	3+09.77
L105	6.45		N20° 33' 21.76"E	3+09.77	3+16.23
C104	36.59	102.000	N10° 16' 40.88"E	3+16.23	3+52.82
L106	101.31		N00° 00' 00.00"E	3+52.82	4+54.14
C105	36.59	102.000	N10° 16' 40.88"W	4+54.14	4+90.73
L107	6.45		N20° 33' 21.76"W	4+90.73	4+97.18
C106	35.16	98.000	N10° 16' 41.68"W	4+97.18	5+32.34
L108	256.14		N00° 00' 01.59"W	5+32.34	7+88.48
C107	18.89	29.560	N18° 18' 19.02"W	7+88.48	8+07.37
C108	22.78	32.533	N16° 33' 01.98"W	8+07.37	8+30.15



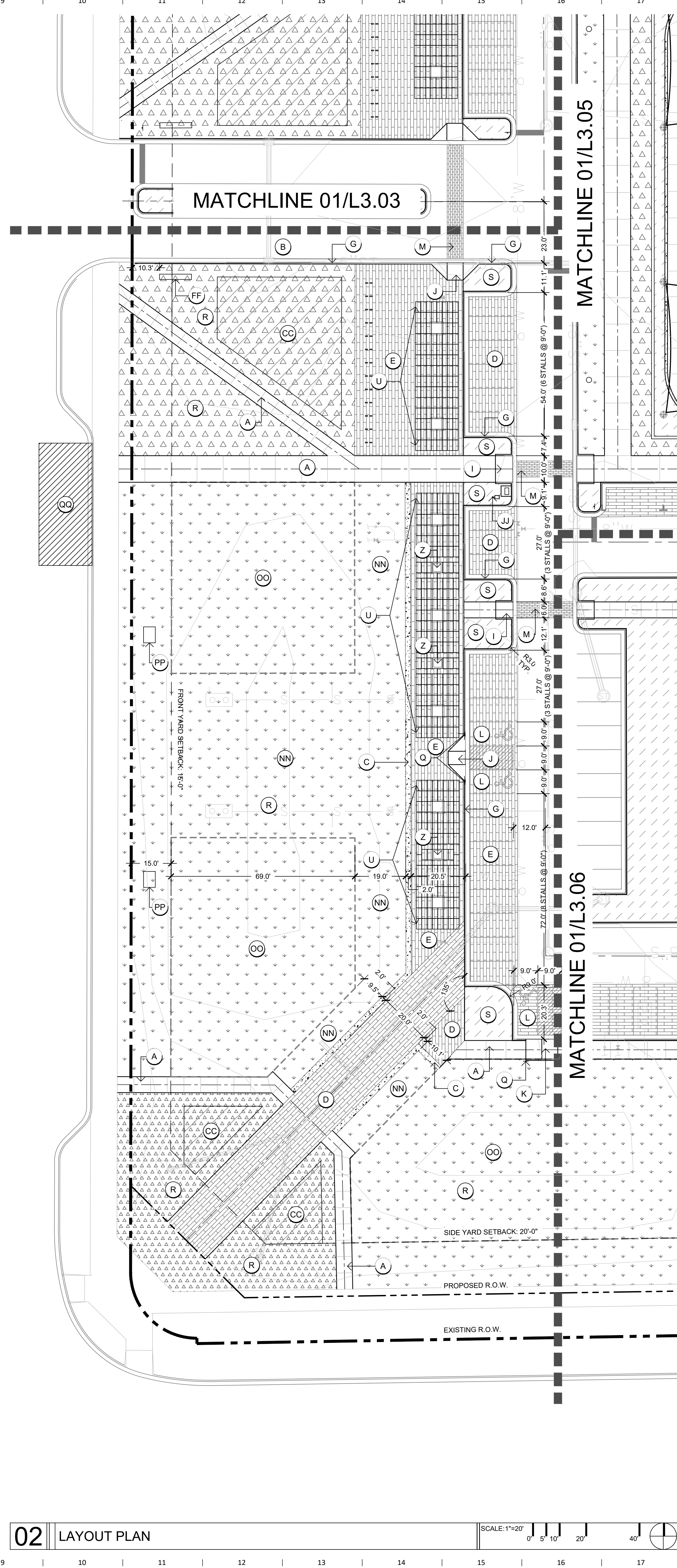
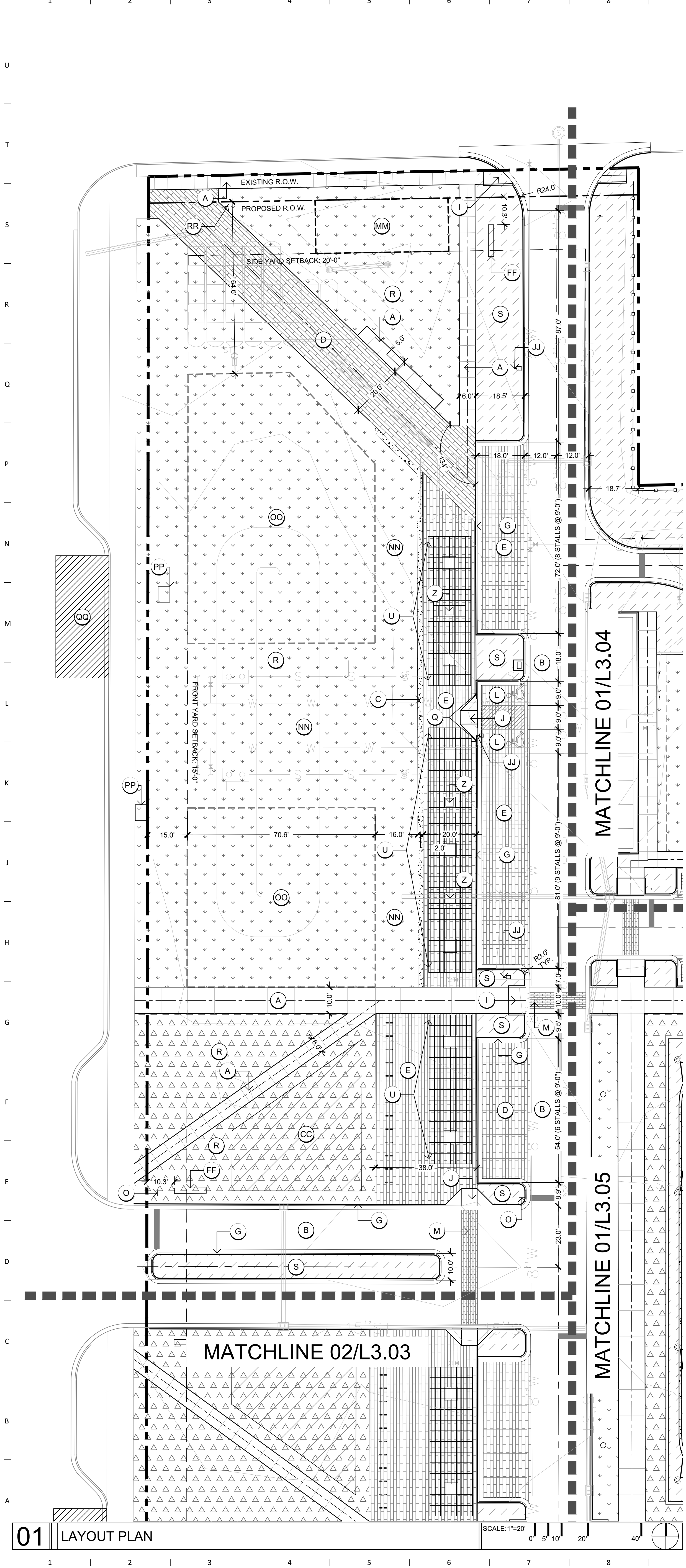
U  
—  
T  
—  
S  
—  
R  
—  
Q  
—  
P  
—  
N  
—  
M  
—  
L  
—  
K  
—  
J  
—  
H  
—  
G  
—  
F  
—  
E  
—  
D  
—  
C  
—  
B  
—  
A

01 LAYOUT PLAN

1 2 3 4 5 6 7 8 9

02 LAYOUT PLAN

10 11 12 13 14 15 16 17



LAYOUT NOTES:

- ALL CURBS RAMP TO BE BUILT AS PER FEDERAL ACCESSIBILITY STANDARDS.
- PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
- BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM SURVEY ARE PREPARED BY CIVIL DESIGN ADVANTAGE, 3405 SE CROSSROADS DR. #G, GRIMES, IA 50111.
- ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
- VERIFY COORDINATES PRIOR TO CONSTRUCTION.
- CONTRACTOR TO SUPPLY AND INSTALL ALL NECESSARY SLEEVES UNDER PAVING AND WALKS.
- PROPOSED SIGNAGE TO MEET CITY CODES FOR GROUND SIGNS.
- CONSTRUCT SITE IMPROVEMENTS IN ACCORDANCE TO THE IOWA SDAS, CURRENT EDITION, AND SPECIFICATIONS.

LEGEND

	TYPE 3 PCC BANDS
	TYPE 4 UNIT PAVER
	TYPE 5 UNIT PAVER
	TURF SEED
	SOD
	NATIVE SEED
	PLANTING BED

LAYOUT PLAN KEYNOTES

- (A) TYPE 1 PCC WALKS, REFER TO DETAIL 01/L6.01  
(B) TYPE 2 PCC DRIVES, REFER TO DETAIL 02/L6.01  
(C) TYPE 3 DECORATIVE PCC BANDS, REFER TO DETAIL 03/L6.01  
(D) TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01  
(E) TYPE 5 PERMEABLE UNIT PAVERS, REFER TO DETAIL 04/L6.01  
(F) TYPE 6 UNIT PAVERS, REFER TO DETAIL 05/L6.01  
(G) STANDARD PCC CURB AND GUTTER, REFER TO DETAIL 17/L6.01  
(H) PCC ROLL CURB, REFER TO DETAIL 19/L6.01  
(I) PERPENDICULAR RETURN CURB RAMP, REFER TO DETAIL 13/L6.01  
(J) PERPENDICULAR FLARED CURB RAMP, REFER TO DETAIL 14/L6.01  
(K) PARALLEL CURB RAMP, REFER TO DETAIL 16/L6.01  
(L) ADA PARKING STALL, REFER TO DETAIL 04/L6.03  
(M) HIGH VISIBILITY CROSS WALK WITH TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01  
(N) TEMPORARY LOADING ZONE SIGN, REFER TO DETAIL 02/L6.02  
(O) STOP SIGN, REFER TO DETAIL 05/L6.02  
(P) DO NOT ENTER SIGN, REFER TO DETAIL 06/L6.02  
(Q) ADA PARKING SIGN, REFER TO DETAIL 01/L6.02  
(R) TURF, REFER TO PLANTING SHEETS  
(S) PLANTING BED, REFER TO PLANTING SHEETS  
(T) NATIVE SEED AREA, REFER TO PLANTING SHEETS  
(U) STRUCTURAL SOIL CELL, REFER TO SHEET L6.04 FOR LAYOUT AND DETAILS  
(V) LANDSCAPE EDGING, REFER TO DETAIL 03/L6.06  
(W) CBU MAILBOX  
(X) LIMESTONE SLAB, STANDARD BUFF, SMOOTH FINISH, REFER TO ARCH SHEETS  
(Y) PCC RAISED PLANTER, REFER TO DETAIL 04/L6.02  
(Z) PCC PLANTER CURB, REFER TO DETAIL 16/L6.06  
(AA) PCC STAIR AND HANDRAIL, REFER TO DETAIL 20/L6.01  
(BB) PCC STOOP, REFER TO DETAIL 08/L6.01  
(CC) POTENTIAL ART LOCATION, MAXIMUM ART HEIGHT OF 15'-0"  
(DD) MOBILE ICE RINK STRUCTURE LOCATION, WINTER MONTHS ONLY  
(EE) SPLASH PAD, BY OTHERS  
(FF) MONUMENT SIGNAGE, 10'-0" MINIMUM SETBACK FROM PROPERTY LINE  
(GG) WOODEN PRIVACY FENCE, REFER TO DETAIL 11/L6.02  
(HH) TRASH ENCLOSURE, REFER TO DETAIL 16/L6.02  
(II) PEDESTRIAN LIGHT BOLLARD, REFER TO ELECTRICAL  
(JJ) AREA LIGHT POLE, REFER TO ELECTRICAL  
(KK) PEDESTRIAN POST TOP FIXTURE, REFER TO ELECTRICAL  
(LL) PROPOSED TRANSFORMER, SEE ELECTRICAL  
(MM) PROPOSED SWITCHGEAR LOCATION, BY OTHERS  
(NN) FUTURE PLAZA SPACE (N.I.C.)  
(OO) FUTURE BUILDING PAD (N.I.C.)  
(PP) FUTURE PROPOSED TRANSFORMER (N.I.C.)  
(QQ) PROPOSED ON-STREET LOADING ZONE  
(RR) CENTRAL IOWA TRAILS WAYFINDING SIGNAGE

100 Court Ave., Suite 100  
Des Moines, IA 50309  
P: 515-309-0722  
F: 515-309-0725  
www.opnarchitects.com

All reports, plans, specifications, computer files, field data, notes and other documents and instruments prepared by OPN Architects, Inc. as instruments of service shall remain the property of OPN Architects, Inc. OPN Architects, Inc. shall retain all common law, statutory and other reserved rights, including the copyright therein.

© 2019 OPN Architects, Inc.

Owner

Project

Landscape Architect

CONFLUENCE  
525 17TH STREET  
DES MOINES, IA 50309  
P. 515-288-4875  
F. 515-288-8359

Civil Engineer

CIVIL DESIGN ADVANTAGE  
3405 SE CROSSROADS DR. #G  
Grimes, IA 50111  
P. 515-369-4400  
F. XXX-XXX-XXXX

Structural Engineer

RAKER RHODES ENGINEERING  
4717 GRAND AVE  
DES MOINES, IA 50312  
P. 515-277-0275  
F. XXX-XXX-XXXX

Mechanical Engineer

IMEG CORP.  
2882 106TH STREET  
DES MOINES, IA 50322  
P. 515-334-9906  
F. 515-334-9908

Electrical Engineer

IMEG CORP.  
2882 106TH STREET  
DES MOINES, IA 50322  
P. 515-334-9906  
F. 515-334-9908

Key Plan

Revision Description Date

NOT FOR  
CONSTRUCTION.

OPN Project No.  
18837000

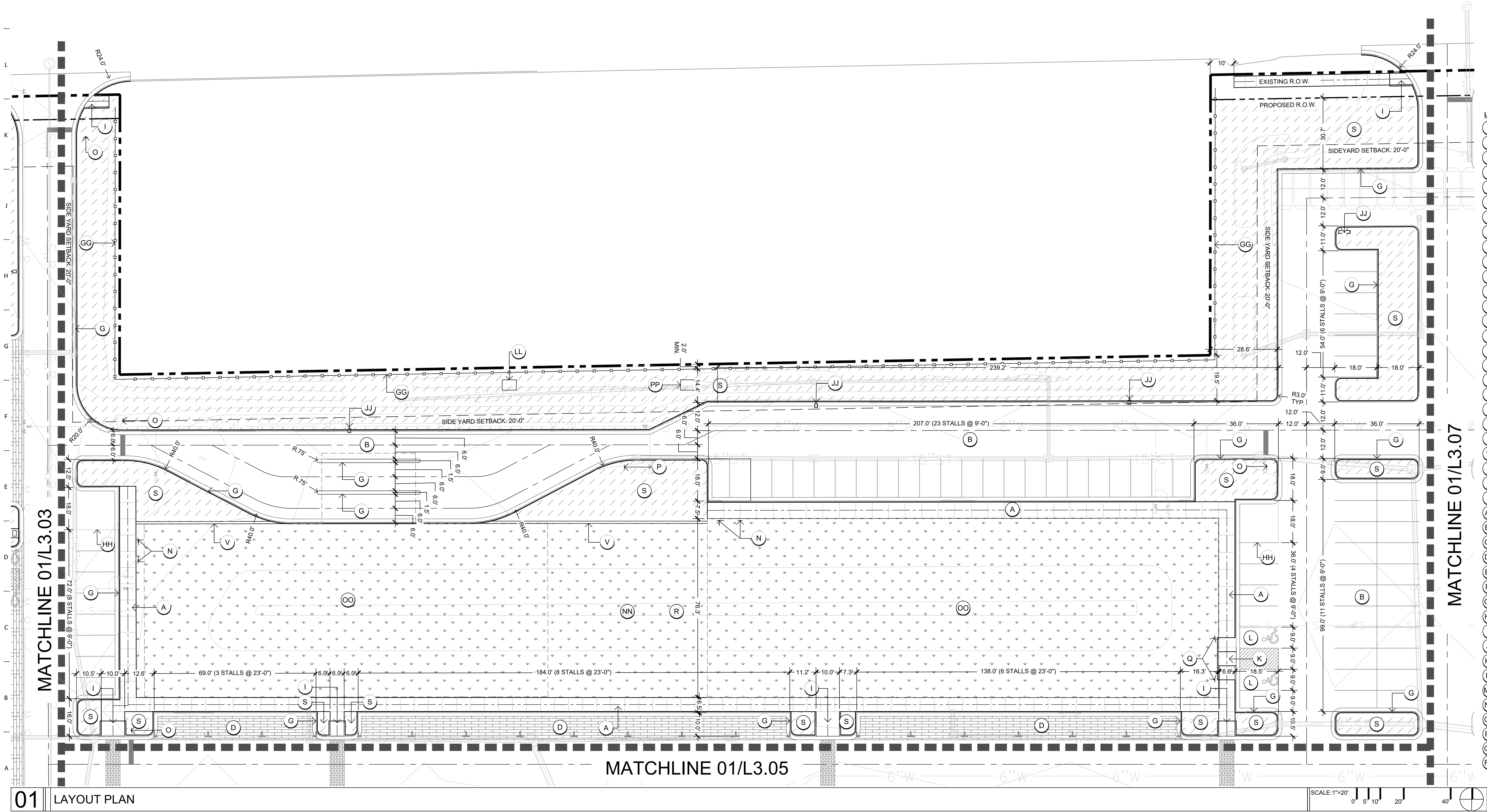
Sheet Issue Date  
SITE PLAN 09/18/19

Sheet Name  
SITE LAYOUT PLAN

Sheet Number



U  
—  
T  
—  
S  
—  
R  
—  
Q  
—  
P  
—  
N  
—  
M  
—  
L  
—  
K  
—  
J  
—  
I  
—  
H  
—  
G  
—  
F  
—  
E  
—  
D  
—  
C  
—  
B  
—  
A



LAYOUT NOTES:

- ALL CURBS RAMPS TO BE BUILT AS PER FEDERAL ACCESSIBILITY STANDARDS.
- PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
- BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM SURVEY ARE PREPARED BY CIVIL DESIGN ADVANTAGE, 3405 SE CROSSROADS DR. #G, GRIMES, IA 50111.
- ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
- VERIFY COORDINATES PRIOR TO CONSTRUCTION.
- CONTRACTOR TO SUPPLY AND INSTALL ALL NECESSARY SLEEVES UNDER PAVING AND WALKS.
- PROPOSED SIGNAGE TO MEET CITY CODES FOR GROUND SIGNS.
- CONSTRUCT SITE IMPROVEMENTS IN ACCORDANCE TO THE IOWA SUDAS, CURRENT EDITION, AND SPECIFICATIONS.

LEGEND

	TYPE 3 PCC BANDS
	TYPE 4 UNIT PAVER
	TYPE 5 UNIT PAVER
	TURF SEED
	SOD
	NATIVE SEED
	PLANTING BED

LAYOUT PLAN KEYNOTES

- (A) TYPE 1 PCC WALKS, REFER TO DETAIL 01/L6.01  
(B) TYPE 2 PCC DRIVES, REFER TO DETAIL 02/L6.01  
(C) TYPE 3 DECORATIVE PCC BANDS, REFER TO DETAIL 03/L6.01  
(D) TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01  
(E) TYPE 5 PERMEABLE UNIT PAVERS, REFER TO DETAIL 04/L6.01  
(F) TYPE 6 UNIT PAVERS, REFER TO DETAIL 05/L6.01  
(G) STANDARD PCC CURB AND GUTTER, REFER TO DETAIL 17/L6.01  
(H) PCC ROLL CURB, REFER TO DETAIL 19/L6.01  
(I) PERPENDICULAR RETURN CURB RAMP, REFER TO DETAIL 13/L6.01  
(J) PERPENDICULAR FLARED CURB RAMP, REFER TO DETAIL 14/L6.01  
(K) PARALLEL CURB RAMP, REFER TO DETAIL 16/L6.01  
(L) ADA PARKING STALL, REFER TO DETAIL 04/L6.03  
(M) HIGH VISIBILITY CROSS WALK WITH TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01  
(N) TEMPORARY LOADING ZONE SIGN, REFER TO DETAIL 02/L6.02  
(O) STOP SIGN, REFER TO DETAIL 05/L6.02  
(P) DO NOT ENTER SIGN, REFER TO DETAIL 06/L6.02  
(Q) ADA PARKING SIGN, REFER TO DETAIL 01/L6.02  
(R) TURF, REFER TO PLANTING SHEETS  
(S) PLANTING BED, REFER TO PLANTING SHEETS  
(T) NATIVE SEED AREA, REFER TO PLANTING SHEETS  
(U) STRUCTURAL SOIL CELL, REFER TO SHEET L6.04 FOR LAYOUT AND DETAILS  
(V) LANDSCAPE EDGING, REFER TO DETAIL 03/L6.06  
(W) CBU MAILBOX  
(X) LIMESTONE SLAB, STANDARD BUFF, SMOOTH FINISH, REFER TO ARCH SHEETS  
(Y) PCC RAISED PLANTER, REFER TO DETAIL 04/L6.02  
(Z) PCC PLANTER CURB, REFER TO DETAIL 16/L6.06  
(AA) PCC STAIR AND HANDRAIL, REFER TO DETAIL 20/L6.01  
(BB) PCC STOOP, REFER TO DETAIL 08/L6.01  
(CC) POTENTIAL ART LOCATION, MAXIMUM ART HEIGHT OF 15'-0"  
(DD) MOBILE ICE RINK STRUCTURE LOCATION, WINTER MONTHS ONLY  
(EE) SPLASH PAD, BY OTHERS  
(FF) MONUMENT SIGNAGE, 10'-0" MINIMUM SETBACK FROM PROPERTY LINE  
(GG) WOODEN PRIVACY FENCE, REFER TO DETAIL 11/L6.02  
(HH) TRASH ENCLOSURE, REFER TO DETAIL 16/L6.02  
(II) PEDESTRIAN LIGHT BOLLARD, REFER TO ELECTRICAL  
(JJ) AREA LIGHT POLE, REFER TO ELECTRICAL  
(KK) PEDESTRIAN POST TOP FIXTURE, REFER TO ELECTRICAL  
(LL) PROPOSED TRANSFORMER, SEE ELECTRICAL  
(MM) PROPOSED SWITCHGEAR LOCATION, BY OTHERS  
(NN) FUTURE PLAZA SPACE (N.I.C.)  
(OO) FUTURE BUILDING PAD (N.I.C.)  
(PP) FUTURE PROPOSED TRANSFORMER (N.I.C.)  
(QQ) PROPOSED ON-STREET LOADING ZONE  
(RR) CENTRAL IOWA TRAILS WAYFINDING SIGNAGE



LAYOUT NOTES:

- ALL CURBS RAMP TO BE BUILT AS PER FEDERAL ACCESSIBILITY STANDARDS.
- PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
- BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM SURVEY ARE PREPARED BY CIVIL DESIGN ADVANTAGE, 3405 SE CROSSROADS DR. #G, GRIMES, IA 50111.
- ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
- VERIFY COORDINATES PRIOR TO CONSTRUCTION.
- CONTRACTOR TO SUPPLY AND INSTALL ALL NECESSARY SLEEVES UNDER PAVING AND WALKS.
- PROPOSED SIGNAGE TO MEET CITY CODES FOR GROUND SIGNS.
- CONSTRUCT SITE IMPROVEMENTS IN ACCORDANCE TO THE IOWA SUDAS, CURRENT EDITION, AND SPECIFICATIONS.

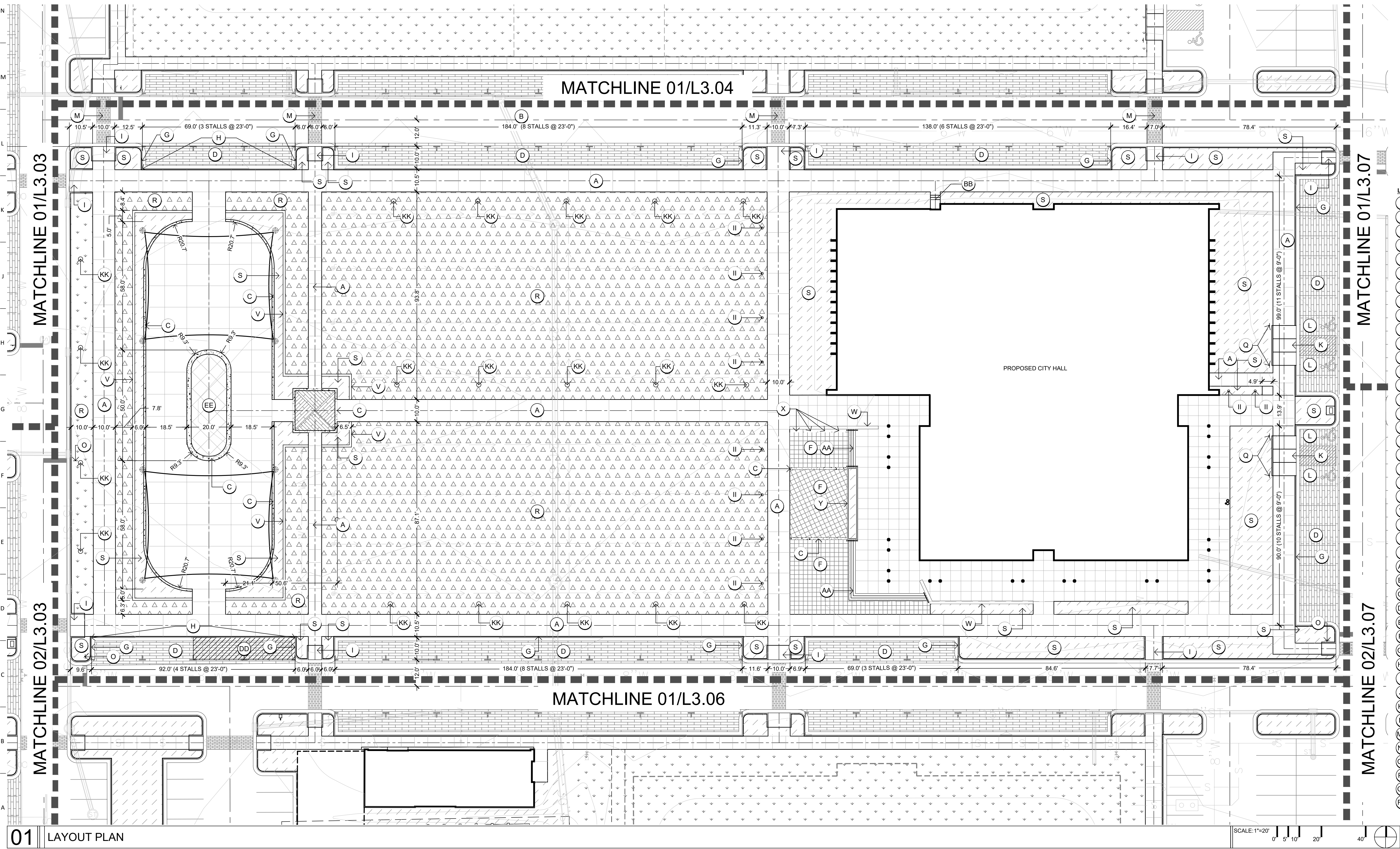
LEGEND

	TYPE 3 PCC BANDS
	TYPE 4 UNIT PAVER
	TYPE 5 UNIT PAVER
	TURF SEED
	SOD
	NATIVE SEED
	PLANTING BED

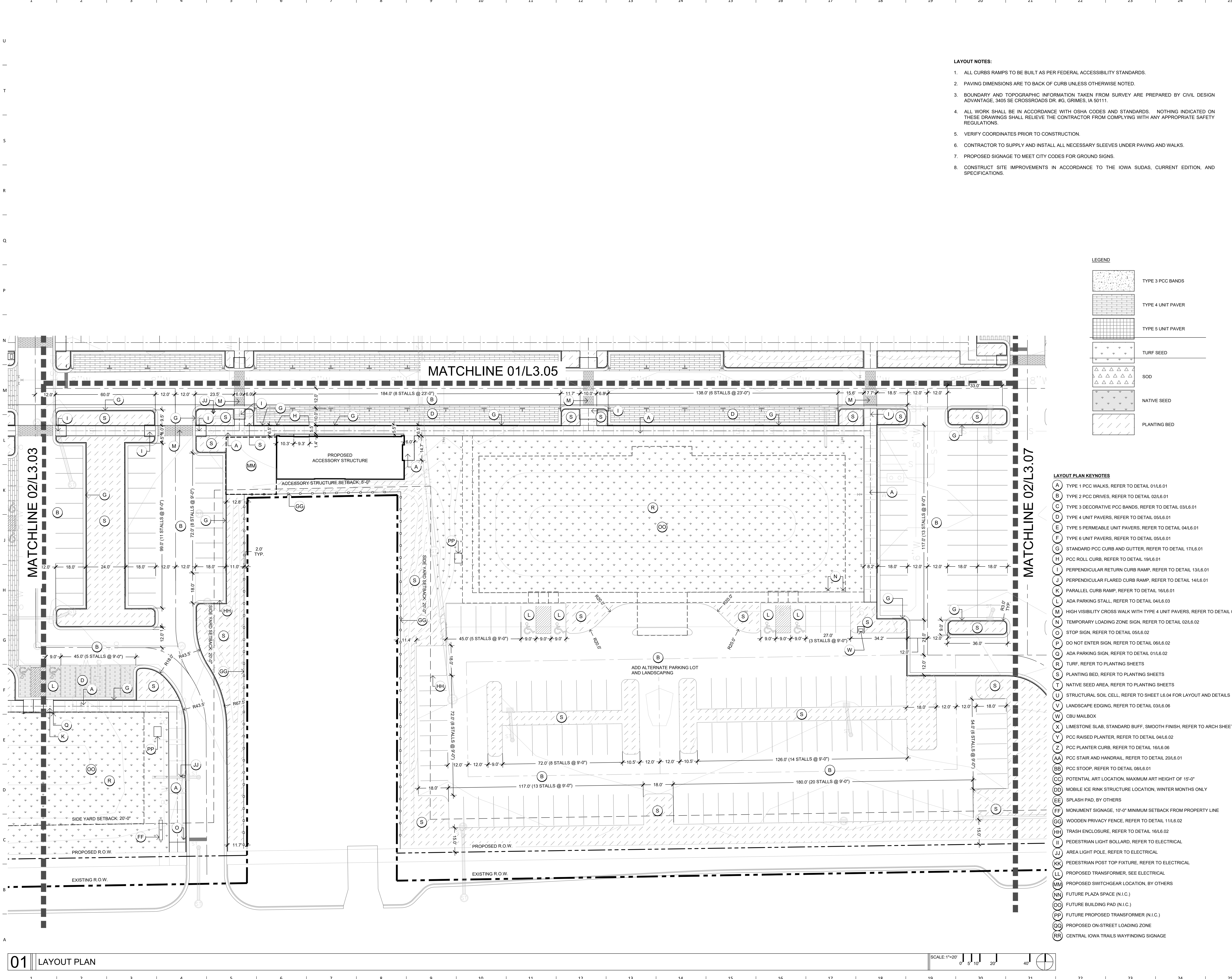
LAYOUT PLAN KEYNOTES

A	TYPE 1 PCC WALKS, REFER TO DETAIL 01/L6.01
B	TYPE 2 PCC DRIVES, REFER TO DETAIL 02/L6.01
C	TYPE 3 DECORATIVE PCC BANDS, REFER TO DETAIL 03/L6.01
D	TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01
E	TYPE 5 PERMEABLE UNIT PAVERS, REFER TO DETAIL 04/L6.01
F	TYPE 6 UNIT PAVERS, REFER TO DETAIL 05/L6.01
G	STANDARD PCC CURB AND GUTTER, REFER TO DETAIL 17/L6.01
H	PCC ROLL CURB, REFER TO DETAIL 19/L6.01
I	PERPENDICULAR RETURN CURB RAMP, REFER TO DETAIL 13/L6.01
L	PERPENDICULAR FLARED CURB RAMP, REFER TO DETAIL 14/L6.01
K	PARALLEL CURB RAMP, REFER TO DETAIL 16/L6.01
L	ADA PARKING STALL, REFER TO DETAIL 04/L6.03
M	HIGH VISIBILITY CROSS WALK WITH TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01
N	TEMPORARY LOADING ZONE SIGN, REFER TO DETAIL 02/L6.02
O	STOP SIGN, REFER TO DETAIL 05/L6.02
P	DO NOT ENTER SIGN, REFER TO DETAIL 06/L6.02
Q	ADA PARKING SIGN, REFER TO DETAIL 01/L6.02
R	TURF, REFER TO PLANTING SHEETS
S	PLANTING BED, REFER TO PLANTING SHEETS
T	NATIVE SEED AREA, REFER TO PLANTING SHEETS
V	STRUCTURAL SOIL CELL, REFER TO SHEET L6.04 FOR LAYOUT AND DETAILS
W	LANDSCAPE EDGING, REFER TO DETAIL 03/L6.06
X	CBU MAILBOX
Y	LIMESTONE SLAB, STANDARD BUFF, SMOOTH FINISH, REFER TO ARCH SHEETS
Z	PCC RAISED PLANTER, REFER TO DETAIL 04/L6.02
AA	PCC PLANTER CURB, REFER TO DETAIL 16/L6.06
BB	PCC STAIR AND HANDRAIL, REFER TO DETAIL 20/L6.01
CC	PCC STOOP, REFER TO DETAIL 08/L6.01
DD	POTENTIAL ART LOCATION, MAXIMUM ART HEIGHT OF 15'-0"
EE	MOBILE ICE RINK STRUCTURE LOCATION, WINTER MONTHS ONLY
FF	SPLASH PAD, BY OTHERS
GG	MONUMENT SIGNAGE, 10'-0" MINIMUM SETBACK FROM PROPERTY LINE
HH	WOODEN PRIVACY FENCE, REFER TO DETAIL 11/L6.02
II	TRASH ENCLOSURE, REFER TO DETAIL 16/L6.02
J	PEDESTRIAN LIGHT BOLLARD, REFER TO ELECTRICAL
KK	AREA LIGHT POLE, REFER TO ELECTRICAL
LL	PEDESTRIAN POST TOP FIXTURE, REFER TO ELECTRICAL
MM	PROPOSED TRANSFORMER, SEE ELECTRICAL
NN	PROPOSED SWITCHGEAR LOCATION, BY OTHERS
OO	FUTURE PLAZA SPACE (N.I.C.)
PP	FUTURE BUILDING PAD (N.I.C.)
QQ	FUTURE PROPOSED TRANSFORMER (N.I.C.)
RR	PROPOSED ON-STREET LOADING ZONE
SS	CENTRAL IOWA TRAILS WAYFINDING SIGNAGE

NOT FOR CONSTRUCTION.







- LAYOUT NOTES:**
- ALL CURBS RAMPS TO BE BUILT AS PER FEDERAL ACCESSIBILITY STANDARDS.
  - PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
  - BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM SURVEY ARE PREPARED BY CIVIL DESIGN ADVANTAGE, 3405 SE CROSSROADS DR. #G, GRIMES, IA 50111.
  - ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
  - VERIFY COORDINATES PRIOR TO CONSTRUCTION.
  - CONTRACTOR TO SUPPLY AND INSTALL ALL NECESSARY SLEEVES UNDER PAVING AND WALKS.
  - PROPOSED SIGNAGE TO MEET CITY CODES FOR GROUND SIGNS.
  - CONSTRUCT SITE IMPROVEMENTS IN ACCORDANCE TO THE IOWA SUDAS, CURRENT EDITION, AND SPECIFICATIONS.

LEGEND	
	TYPE 3 PCC BANDS
	TYPE 4 UNIT PAVER
	TYPE 5 UNIT PAVER
	TURF SEED
	SOD
	NATIVE SEED
	PLANTING BED

- LAYOUT PLAN KEYNOTES**
- A TYPE 1 PCC WALKS, REFER TO DETAIL 01/L6.01
  - B TYPE 2 PCC DRIVES, REFER TO DETAIL 02/L6.01
  - C TYPE 3 DECORATIVE PCC BANDS, REFER TO DETAIL 03/L6.01
  - D TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01
  - E TYPE 5 PERMEABLE UNIT PAVERS, REFER TO DETAIL 04/L6.01
  - F TYPE 6 UNIT PAVERS, REFER TO DETAIL 05/L6.01
  - G STANDARD PCC CURB AND GUTTER, REFER TO DETAIL 17/L6.01
  - H PCC ROLL CURB, REFER TO DETAIL 19/L6.01
  - I PERPENDICULAR RETURN CURB RAMP, REFER TO DETAIL 13/L6.01
  - L PERPENDICULAR FLARED CURB RAMP, REFER TO DETAIL 14/L6.01
  - K PARALLEL CURB RAMP, REFER TO DETAIL 16/L6.01
  - L ADA PARKING STALL, REFER TO DETAIL 04/L6.03
  - M HIGH VISIBILITY CROSS WALK WITH TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01
  - N TEMPORARY LOADING ZONE SIGN, REFER TO DETAIL 02/L6.02
  - O STOP SIGN, REFER TO DETAIL 05/L6.02
  - P DO NOT ENTER SIGN, REFER TO DETAIL 06/L6.02
  - Q ADA PARKING SIGN, REFER TO DETAIL 01/L6.02
  - R TURF, REFER TO PLANTING SHEETS
  - S PLANTING BED, REFER TO PLANTING SHEETS
  - T NATIVE SEED AREA, REFER TO PLANTING SHEETS
  - U STRUCTURAL SOIL CELL, REFER TO SHEET L6.04 FOR LAYOUT AND DETAILS
  - V LANDSCAPE EDGING, REFER TO DETAIL 03/L6.06
  - W CBU MAILBOX
  - X LIMESTONE SLAB, STANDARD BUFF, SMOOTH FINISH, REFER TO ARCH SHEETS
  - Y PCC RAISED PLANTER, REFER TO DETAIL 04/L6.02
  - Z PCC PLANTER CURB, REFER TO DETAIL 16/L6.06
  - AA PCC STAIR AND HANDRAIL, REFER TO DETAIL 20/L6.01
  - BB PCC STOOP, REFER TO DETAIL 08/L6.01
  - CC POTENTIAL ART LOCATION, MAXIMUM ART HEIGHT OF 15'-0"
  - DD MOBILE ICE RINK STRUCTURE LOCATION, WINTER MONTHS ONLY
  - EE SPLASH PAD, BY OTHERS
  - FF MONUMENT SIGNAGE, 10'-0" MINIMUM SETBACK FROM PROPERTY LINE
  - GG WOODEN PRIVACY FENCE, REFER TO DETAIL 11/L6.02
  - HH TRASH ENCLOSURE, REFER TO DETAIL 16/L6.02
  - II PEDESTRIAN LIGHT BOLLARD, REFER TO ELECTRICAL
  - JJ AREA LIGHT POLE, REFER TO ELECTRICAL
  - KK PEDESTRIAN POST TOP FIXTURE, REFER TO ELECTRICAL
  - LL PROPOSED TRANSFORMER, SEE ELECTRICAL
  - MM PROPOSED SWITCHGEAR LOCATION, BY OTHERS
  - NN FUTURE PLAZA SPACE (N.I.C.)
  - OO FUTURE BUILDING PAD (N.I.C.)
  - PP FUTURE PROPOSED TRANSFORMER (N.I.C.)
  - QQ PROPOSED ON-STREET LOADING ZONE
  - RR CENTRAL IOWA TRAILS WAYFINDING SIGNAGE

OPNARCHITECTS

100 Court Ave., Suite 100  
Des Moines, IA 50309  
P: 515-309-0722  
F: 515-309-0725  
www.opnarchitects.com

All reports, plans, specifications, computer files, field data, notes and other documents and instruments prepared by OPN Architects, Inc. as instruments of service shall remain the property of OPN Architects, Inc. OPN Architects, Inc. shall retain all common law, statutory and other reserved rights, including the copyright therein.

© 2019 OPN Architects, Inc.

Owner

Project

Landscape Architect

CONFLUENCE  
525 17TH STREET  
DES MOINES, IA 50309  
P. 515-288-4875  
F. 515-288-8359

Civil Engineer

CIVIL DESIGN ADVANTAGE  
3405 SE CROSSROADS DR. #G  
Grimes, IA 50111  
P. 515-369-4400  
F. XXX-XXX-XXXX

Structural Engineer

RAKER RHODES ENGINEERING  
4717 GRAND AVE  
DES MOINES, IA 50312  
P. 515-277-0275  
F. XXX-XXX-XXXX

Mechanical Engineer

IMEG CORP.  
2882 106TH STREET  
DES MOINES, IA 50322  
P. 515-334-9906  
F. 515-334-9908

Electrical Engineer

IMEG CORP.  
2882 106TH STREET  
DES MOINES, IA 50322  
P. 515-334-9906  
F. 515-334-9908

Key Plan

Revision Description Date

NOT FOR CONSTRUCTION.

OPN Project No.  
18837000

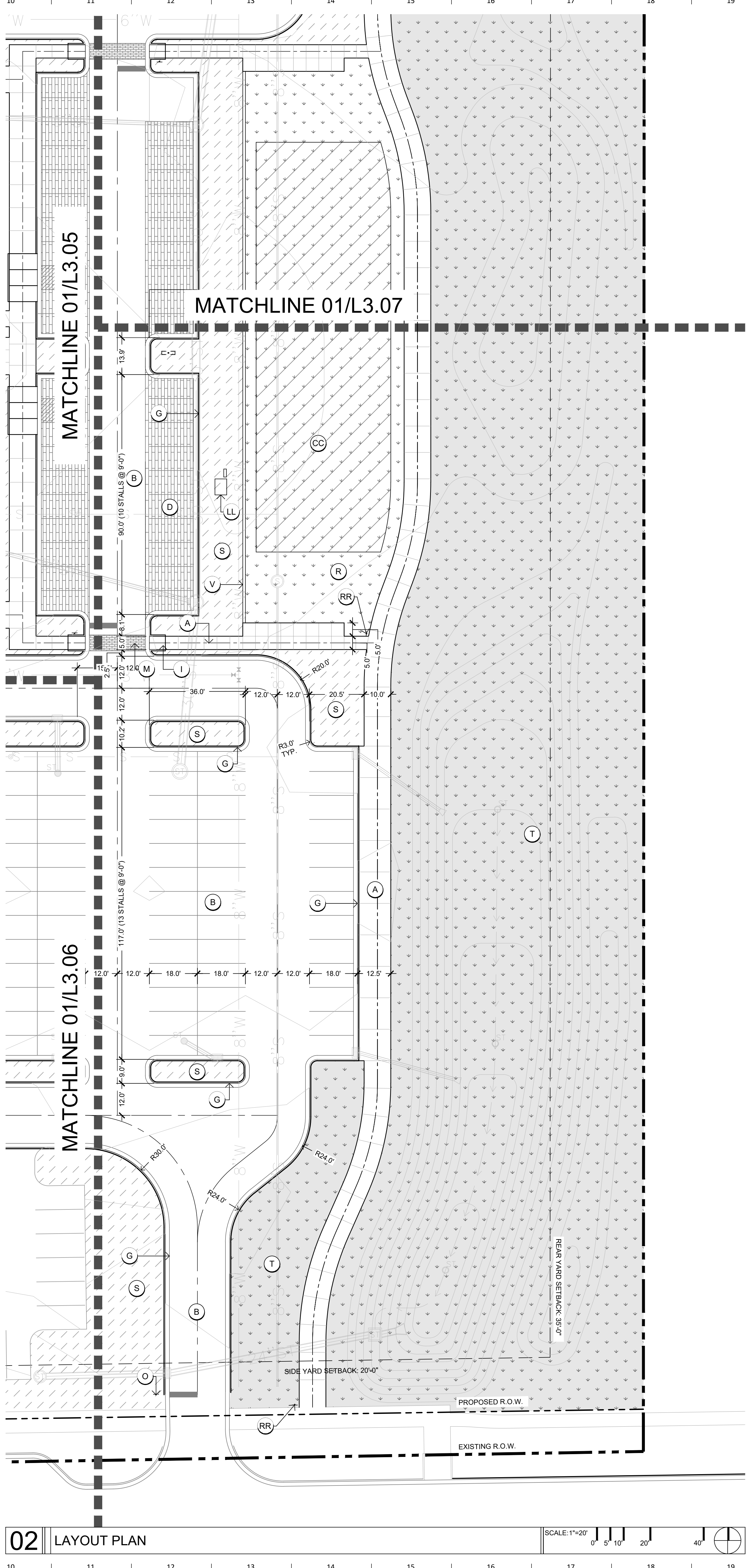
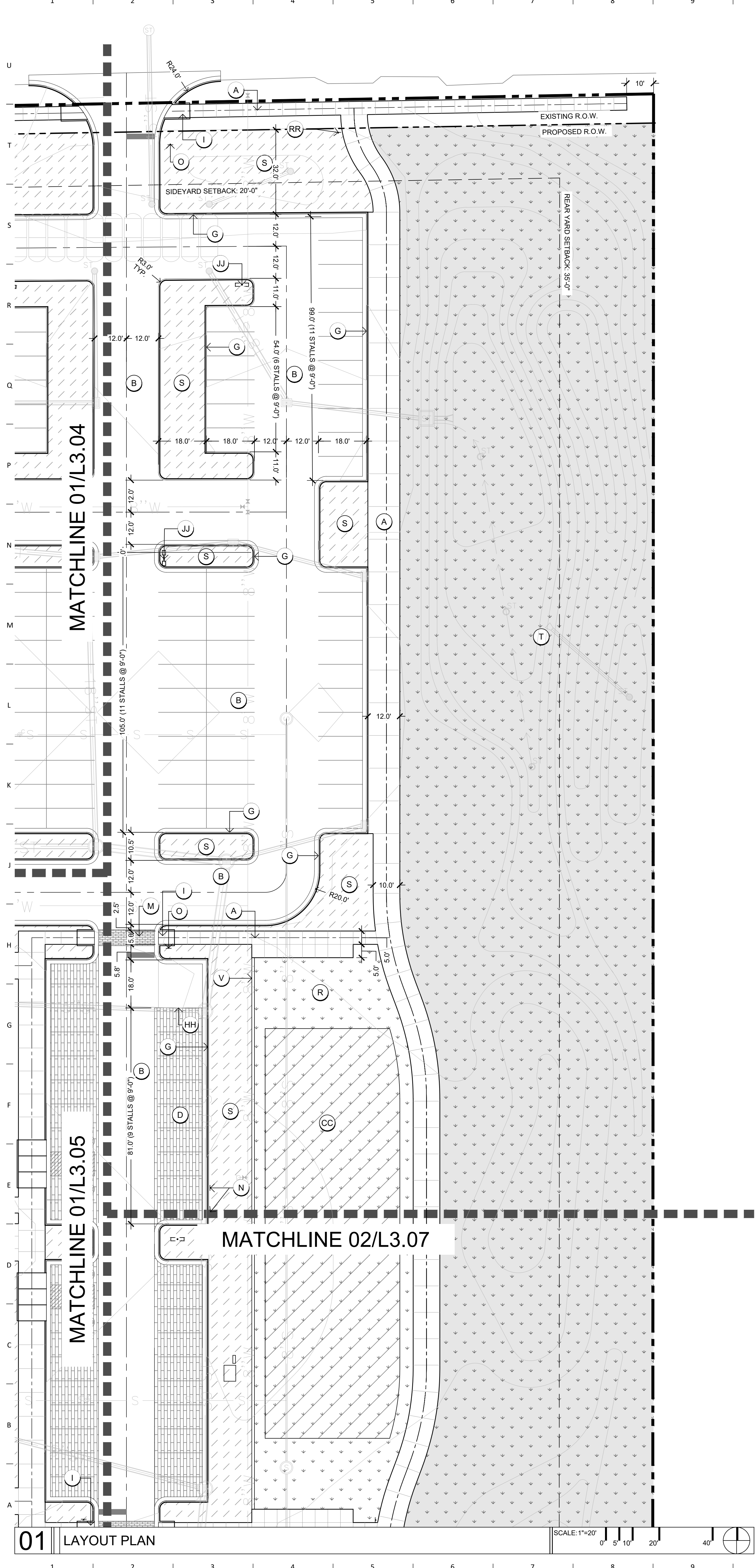
Sheet Issue Date  
09/18/19

Sheet Name  
SITE LAYOUT PLAN

Sheet Number

L3.06





LAYOUT NOTES:

- ALL CURBS RAMPS TO BE BUILT AS PER FEDERAL ACCESSIBILITY STANDARDS.
- PAVING DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
- BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM SURVEY ARE PREPARED BY CIVIL DESIGN ADVANTAGE, 3405 SE CROSSROADS DR. #G, GRIMES, IA 50111.
- ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS.
- VERIFY COORDINATES PRIOR TO CONSTRUCTION.
- CONTRACTOR TO SUPPLY AND INSTALL ALL NECESSARY SLEEVES UNDER PAVING AND WALKS.
- PROPOSED SIGNAGE TO MEET CITY CODES FOR GROUND SIGNS.
- CONSTRUCT SITE IMPROVEMENTS IN ACCORDANCE TO THE IOWA SUDAS, CURRENT EDITION, AND SPECIFICATIONS.

LEGEND

	TYPE 3 PCC BANDS
	TYPE 4 UNIT PAVER
	TYPE 5 UNIT PAVER
	TURF SEED
	SOD
	NATIVE SEED
	PLANTING BED

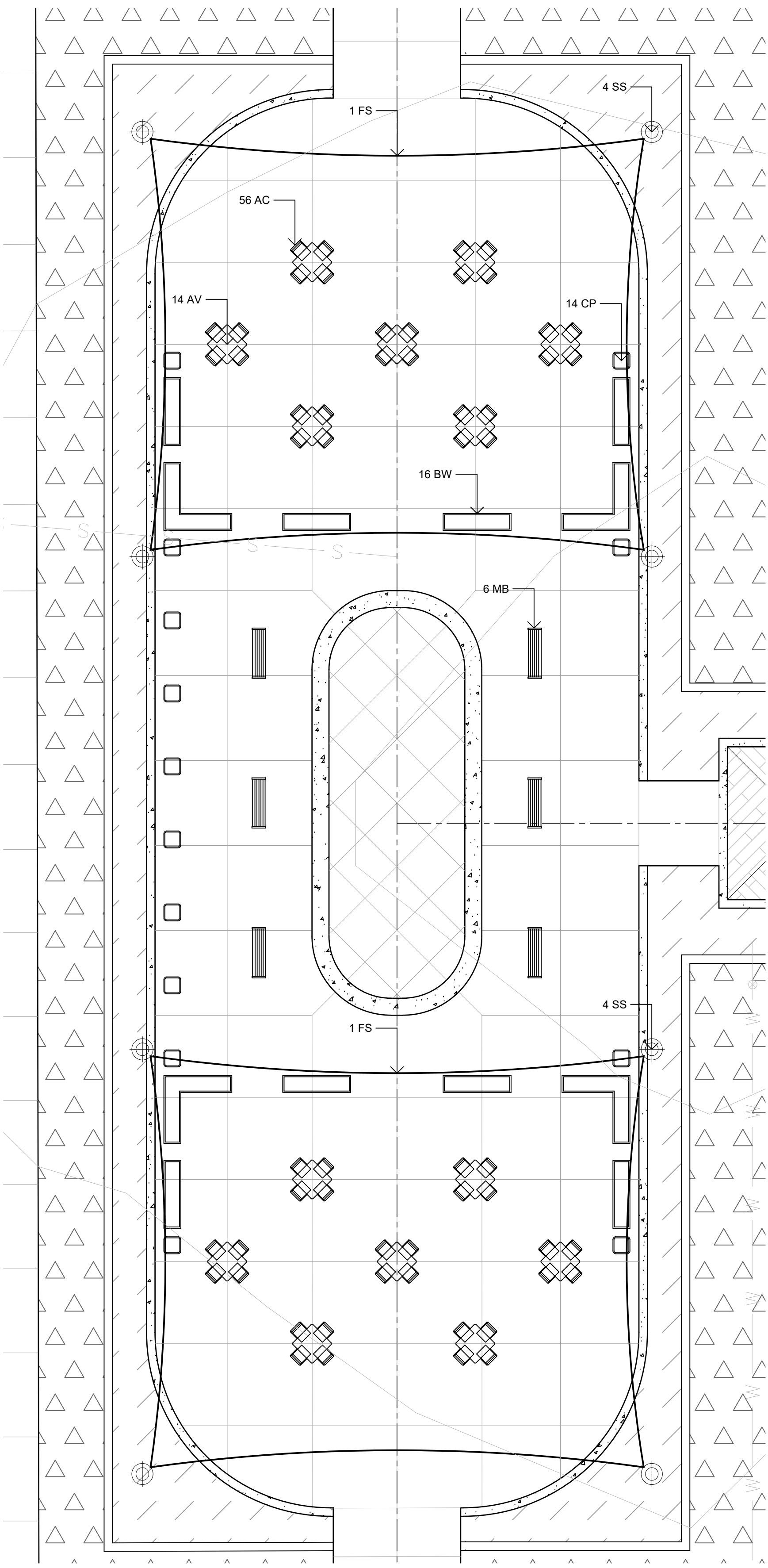
LAYOUT PLAN KEYNOTES

- (A) TYPE 1 PCC WALKS, REFER TO DETAIL 01/L6.01
- (B) TYPE 2 PCC DRIVES, REFER TO DETAIL 02/L6.01
- (C) TYPE 3 DECORATIVE PCC BANDS, REFER TO DETAIL 03/L6.01
- (D) TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01
- (E) TYPE 5 PERMEABLE UNIT PAVERS, REFER TO DETAIL 04/L6.01
- (F) TYPE 6 UNIT PAVERS, REFER TO DETAIL 05/L6.01
- (G) STANDARD PCC CURB AND GUTTER, REFER TO DETAIL 17/L6.01
- (H) PCC ROLL CURB, REFER TO DETAIL 19/L6.01
- (I) PERPENDICULAR RETURN CURB RAMP, REFER TO DETAIL 13/L6.01
- (J) PERPENDICULAR FLARED CURB RAMP, REFER TO DETAIL 14/L6.01
- (K) PARALLEL CURB RAMP, REFER TO DETAIL 16/L6.01
- (L) ADA PARKING STALL, REFER TO DETAIL 04/L6.03
- (M) HIGH VISIBILITY CROSS WALK WITH TYPE 4 UNIT PAVERS, REFER TO DETAIL 05/L6.01
- (N) TEMPORARY LOADING ZONE SIGN, REFER TO DETAIL 02/L6.02
- (O) STOP SIGN, REFER TO DETAIL 05/L6.02
- (P) DO NOT ENTER SIGN, REFER TO DETAIL 06/L6.02
- (Q) ADA PARKING SIGN, REFER TO DETAIL 01/L6.02
- (R) TURF, REFER TO PLANTING SHEETS
- (S) PLANTING BED, REFER TO PLANTING SHEETS
- (T) NATIVE SEED AREA, REFER TO PLANTING SHEETS
- (U) STRUCTURAL SOIL CELL, REFER TO SHEET L6.04 FOR LAYOUT AND DETAILS
- (V) LANDSCAPE EDGING, REFER TO DETAIL 03/L6.06
- (W) CBU MAILBOX
- (X) LIMESTONE SLAB, STANDARD BUFF, SMOOTH FINISH, REFER TO ARCH SHEETS
- (Y) PCC RAISED PLANTER, REFER TO DETAIL 04/L6.02
- (Z) PCC PLANTER CURB, REFER TO DETAIL 16/L6.06
- (AA) PCC STAIR AND HANDRAIL, REFER TO DETAIL 20/L6.01
- (BB) PCC STOOP, REFER TO DETAIL 08/L6.01
- (CC) POTENTIAL ART LOCATION, MAXIMUM ART HEIGHT OF 15'-0"
- (DD) MOBILE ICE RINK STRUCTURE LOCATION, WINTER MONTHS ONLY
- (EE) SPLASH PAD, BY OTHERS
- (FF) MONUMENT SIGNAGE, 10'-0" MINIMUM SETBACK FROM PROPERTY LINE
- (GG) WOODEN PRIVACY FENCE, REFER TO DETAIL 11/L6.02
- (HH) TRASH ENCLOSURE, REFER TO DETAIL 16/L6.02
- (II) PEDESTRIAN LIGHT BOLLARD, REFER TO ELECTRICAL
- (JJ) AREA LIGHT POLE, REFER TO ELECTRICAL
- (KK) PEDESTRIAN POST TOP FIXTURE, REFER TO ELECTRICAL
- (LL) PROPOSED TRANSFORMER, SEE ELECTRICAL
- (MM) PROPOSED SWITCHGEAR LOCATION, BY OTHERS
- (NN) FUTURE PLAZA SPACE (N.I.C.)
- (OO) FUTURE BUILDING PAD (N.I.C.)
- (PP) FUTURE PROPOSED TRANSFORMER (N.I.C.)
- (QQ) PROPOSED ON-STREET LOADING ZONE
- (RR) CENTRAL IOWA TRAILS WAYFINDING SIGNAGE

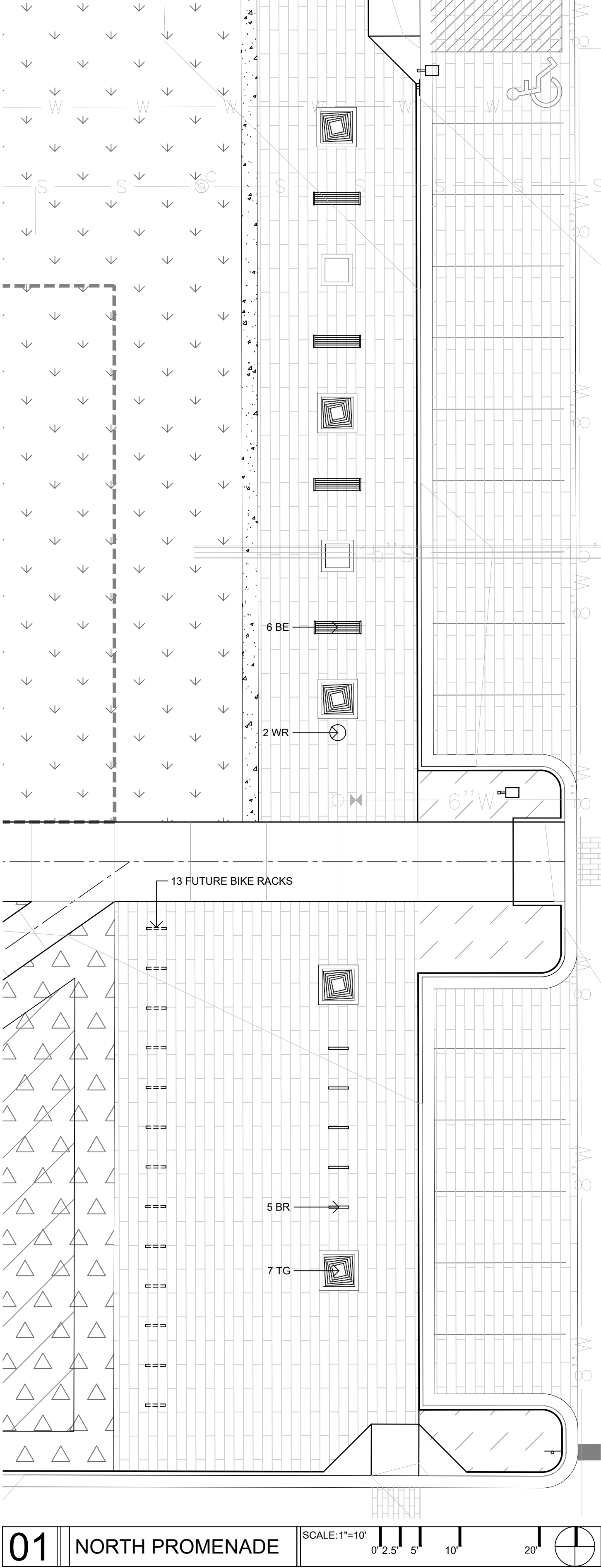
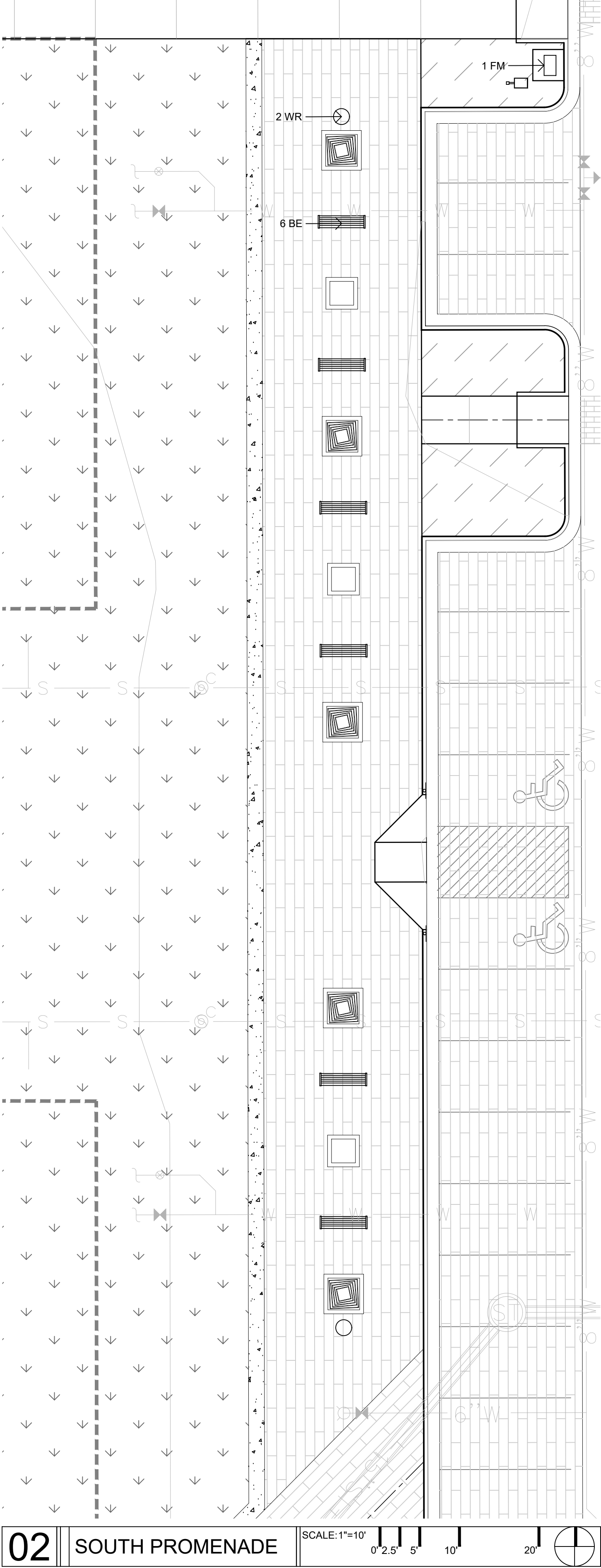


SITE FURNISHINGS					
KEY	QTY.	DESCRIPTION	MANUFACTURER	MODEL	OPTIONS, COLORS AND/OR NOTES
BE	12	BENCH	FORMS + SURFACES	SBBWK-72AC-SFM-2D	MOUNTING: SURFACE MOUNT COLOR: ALUMINUM OPTION: SEAT DIVIDERS
MB	6	MODULAR BENCH	FORMS + SURFACES	SBBWK-72AC-FRS-2D	MOUNTING: FREESTANDING COLOR: ALUMINUM OPTION: SEAT DIVIDERS
AV	22	MODULAR TABLE	FORMS+SURFACES	STAVO-C36R	MOUNTING: FREESTANDING COLOR: ALUMINUM ALUMINUM INSET COLOR: DARK GREY METALLIC
AC	88	MODULAR CHAIR	FORMS+SURFACES	SCAVO	INSET PATTERN: RIVA COLOR: ALUMINUM PATTERN: RIVA
WR	5	WASTE RECEPTACLE	FORMS+SURFACES	SLUNN-36SSS	LID MATERIAL: PE, SLATE BODY FINISH: STAINLESS STEEL, SANDSTONE FINISH & FADE PERFORATION BASE MATERIAL: PE, SLATE MOUNTING: SURFACE LINERS: TWO (2) HALF LINERS W/ BAG STRAPS & DRAIN HOLES LID: SIDE OPENING (STANDARD/STANDARD) LID GRAPHICS: 1) NO MESSAGE 2) RECYCLE/BLUE BACKGROUND
DF	1	DRINKING FOUNTAIN	HAWS	3611FR-01	REFER TO PLUMBING DRAWINGS
BR	36	BIKE RACKS	MM CITE	STE 111	MOUNTING: SURFACE MOUNT COLOR: BLACK
BW	16	BOULEVARD WOOD PLANTER	TOURNESOL	BV-962424	MATERIAL: STANDARD (FRP/ASH WOOD)
CP	18	CONCRETE PLANTER	CREATED IN JOHNSTON	24X24X48"	
TG	14	VERTIGO TREE GRATE	IRONSMITH	4898	MODEL: VERTIGO FRAME: 4800 F FINISH: BLACK POWDER COAT TREE OPENING: 19"
FS	2	4-POINT SHADE SAIL	USA SHADE		REFER TO DETAIL 03/L6.05
SS	8	SHADE SAIL POLES	USA SHADE		REFER TO DETAIL 02/L6.03
BO	4	BOLLARD	DUMOR	400-36	MOUNTING: EMBEDMENT COLOR: CHARCOAL EXPOSED HGT: 35-0"
FG	1	FLAG POLE	POLE TECH CO.		FINISH: ANODIZED BLACK (#335) FLAG SIZE: PER MANUFACTURER
FI	3	BIKE FIX IT STATION	DERO	FIX-IT	MOUNTING: SURFACE MOUNT FINISH: STAINLESS STEEL SATIN FINISH OPTIONS: ADD AIR KIT 2 PUMP MODEL: VITAL
FM	4	CBU MAILBOX	FLORENCE MAILBOXES	1570-8	MOUNTING: SURFACE MOUNT COLOR: BLACK
CB	2	EMERGENCY CALL BOX	CODE BLUE	CB1-S	COLOR: BLACK

03 NORTHWEST BIKE STATION



04 CENTRAL PLAZA



OPNARCHITECTS

100 Court Ave., Suite 100  
Des Moines, IA 50309  
P: 515-309-0722  
F: 515-309-0725  
www.opnarchitects.com

All reports, plans, specifications, computer files, field data, notes and other documents and instruments prepared by OPN Architects, Inc. as instruments of service shall remain the property of OPN Architects, Inc. OPN Architects, Inc. shall retain all common law, statutory and other reserved rights, including the copyright therein.

© 2019 OPN Architects, Inc.

Owner

Project

Landscape Architect

CONFLUENCE  
525 17TH STREET  
DES MOINES, IA 50309  
P. 515-288-4875  
F. 515-288-8359

Civil Engineer

CIVIL DESIGN ADVANTAGE  
3405 SE CROSSROADS DR. #G  
Grimes, IA 50111  
P. 515-369-4400  
F. XXX-XXX-XXXX

Structural Engineer

RAKER RHODES ENGINEERING  
4717 GRAND AVE  
DES MOINES, IA 50312  
P. 515-277-0275  
F. XXX-XXX-XXXX

Mechanical Engineer

IMEG CORP.  
2882 106TH STREET  
DES MOINES, IA 50322  
P. 515-334-9906  
F. 515-334-9908

Electrical Engineer

IMEG CORP.  
2882 106TH STREET  
DES MOINES, IA 50322  
P. 515-334-9906  
F. 515-334-9908

Key Plan

Revision Description Date

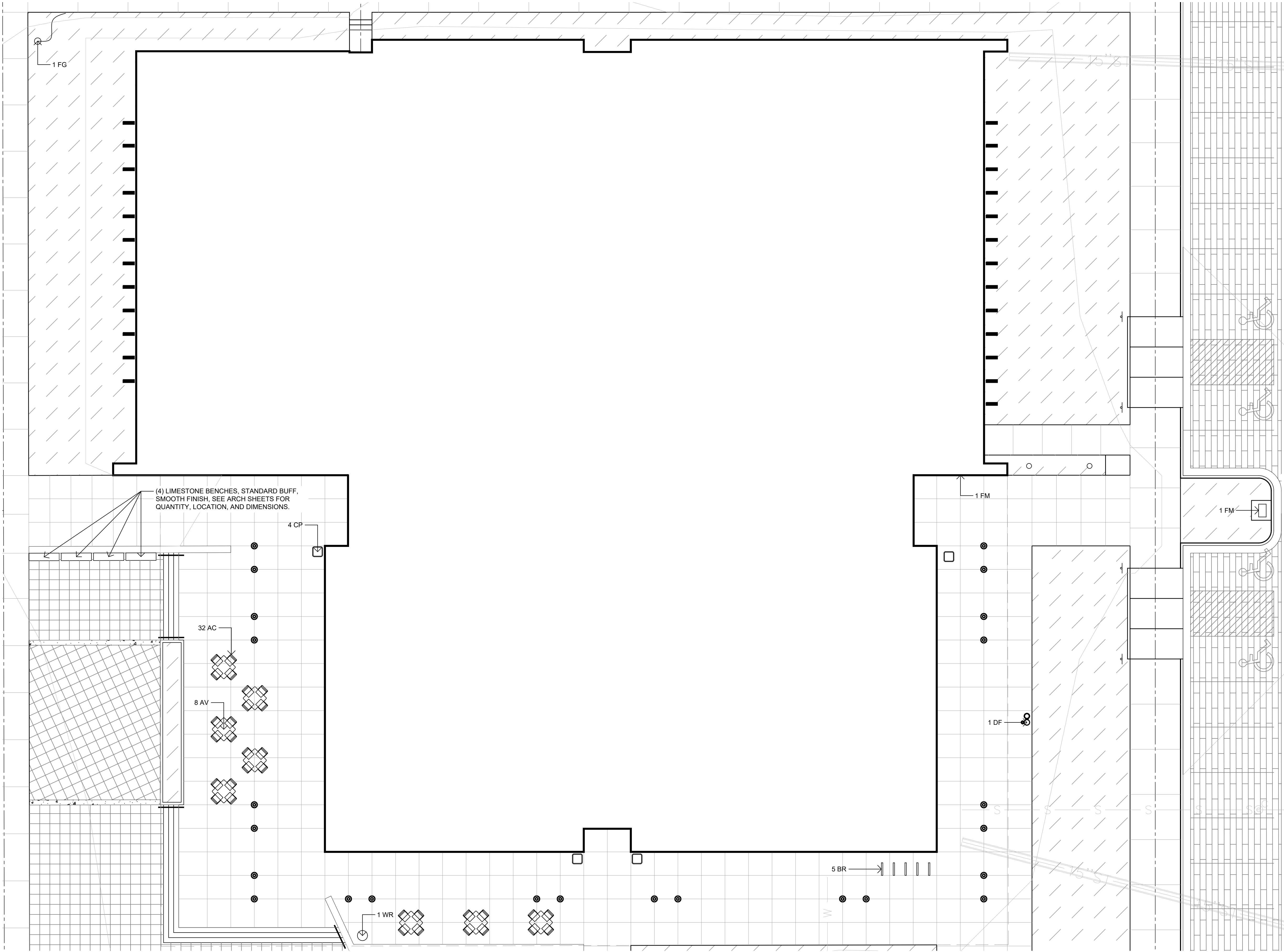
NOT FOR CONSTRUCTION.

OPN Project No.  
18837000  
Sheet Issue Date  
SITE PLAN 09/18/19  
Sheet Name  
SITE FURNISHING PLAN  
Sheet Number

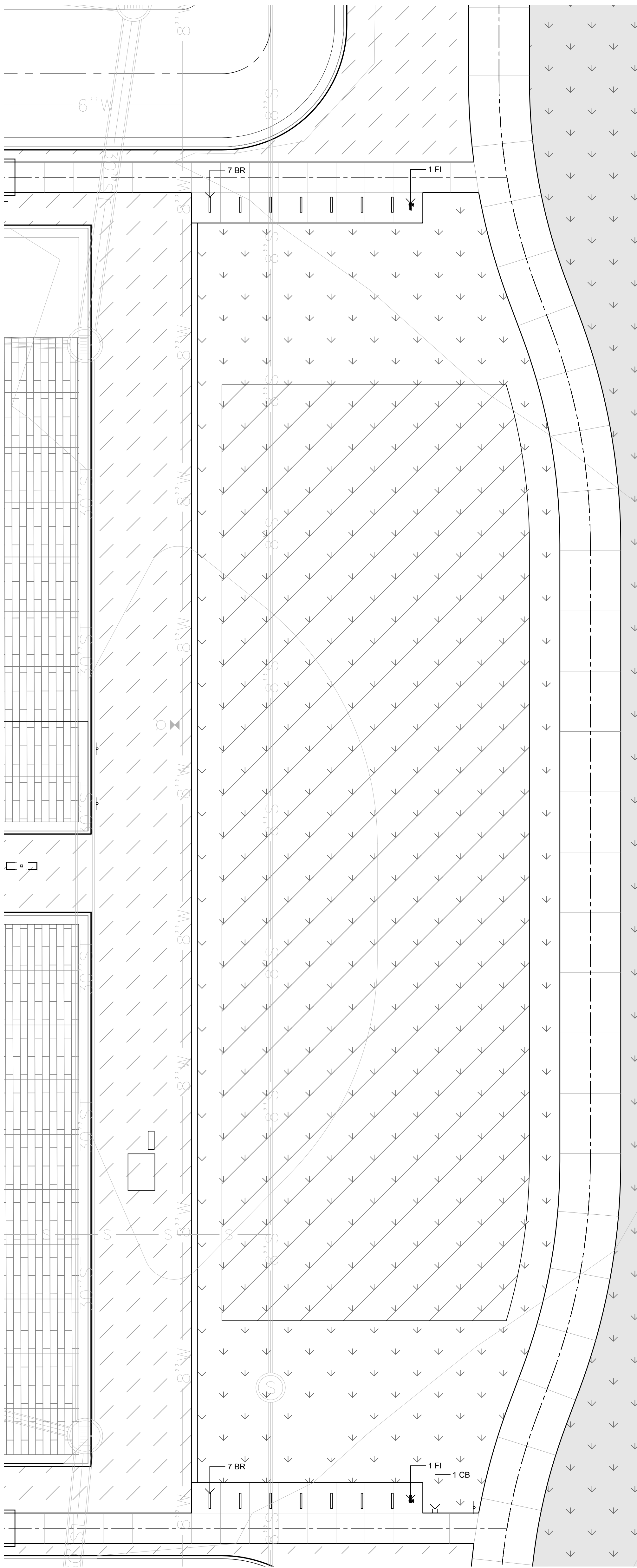


U  
—  
T  
—  
S  
—  
R  
—  
Q  
—  
P  
—  
N  
—  
M  
—  
L  
—  
K  
—  
J  
—  
H  
—  
G  
—  
F  
—  
E  
—  
D  
—  
C  
—  
B  
—  
A

SITE FURNISHINGS					
KEY	QTY.	DESCRIPTION	MANUFACTURER	MODEL	OPTIONS, COLORS AND/OR NOTES
BE	12	BENCH	FORMS + SURFACES	SBWVK-72AC-SFM-2D	MOUNTING: SURFACE MOUNT COLOR: ALUMINUM OPTION: SEAT DIVIDERS
MB	6	MODULAR BENCH	FORMS + SURFACES	SBWVK-72AC-FRS-2D	MOUNTING: FREESTANDING COLOR: ALUMINUM OPTION: SEAT DIVIDERS
AV	22	MODULAR TABLE	FORMS+SURFACES	STAVO-C36R	MOUNTING: FREESTANDING COLOR: ALUMINUM ALUMINUM INSET COLOR: DARK GREY METALLIC INSET PATTERN: RIVA
AC	88	MODULAR CHAIR	FORMS+SURFACES	SCAVO	COLOR: ALUMINUM PATTERN: RIVA
WR	5	WASTE RECEPTACLE	FORMS+SURFACES	SLUNN-36SSS	LID MATERIAL: PE, SLATE BODY FINISH: STAINLESS STEEL, SANDSTONE FINISH & FADE PERFORATION BASE MATERIAL: PE, SLATE MOUNTING: SURFACE LINERS: TWO (2) HALF LINERS W/ BAG STRAPS & DRAIN HOLES LID: SIDE OPENING (STANDARD/STANDARD) LID GRAPHICS: 1) NO MESSAGE 2) RECYCLE/BLUE BACKGROUND
DF	1	DRINKING FOUNTAIN	HAWS	3611FR-01	REFER TO PLUMBING DRAWINGS
BR	36	BIKE RACKS	MM CITE	STE 111	MOUNTING: SURFACE MOUNT COLOR: BLACK
BW	16	BOULEVARD WOOD PLANTER	TOURNESOL	BV-962424	MATERIAL: STANDARD (FRP/ASH WOOD)
CP	18	CONCRETE PLANTER	CREATED IN JOHNSTON	24X24X48"	
TG	14	VERTIGO TREE GRATE	IRONSMITH	4898	MODEL: VERTIGO FRAME: 4800 F FINISH: BLACK POWDER COAT TREE OPENING: 19"
FS	2	4-POINT SHADE SAIL	USA SHADE		REFER TO DETAIL 03/L6.05
SS	8	SHADE SAIL POLES	USA SHADE		REFER TO DETAIL 02/L6.03
BO	4	BOLLARD	DUMOR	400-36	
FG	1	FLAG POLE	POLE TECH CO.		MOUNTING: EMBEDMENT COLOR: CHARCOAL EXPOSED HGT: 35'-0" FINISH: ANODIZED BLACK (#335) FLAG SIZE: PER MANUFACTURER
FI	3	BIKE FIX IT STATION	DERO	FIX-IT	MOUNTING: SURFACE MOUNT FINISH: STAINLESS STEEL SATIN FINISH OPTIONS: ADD AIR KIT 2 PUMP MODEL: VITAL
FM	4	CBU MAILBOX	FLORENCE MAILBOXES	1570-8	MOUNTING: SURFACE MOUNT COLOR: BLACK
CB	2	EMERGENCY CALL BOX	CODE BLUE	CB1-S	COLOR: BLACK



02 CITY HALL TERRACE



01 EAST PARK